

## A. SITE DEVELOPMENT

Standards for each development subarea shall apply to site development in regard to building placement, setbacks, pedestrian zone improvements, landscaping, screening, and the execution of auto-oriented uses. Areas delineating each subarea district for the corridors can be found in Appendix B of the Standards.

### SETBACKS

The corridor roads leading to Urbana's historic town center have a variety of development characteristics. These characteristics range from a more large lot development pattern in the farther extents of corridor roads to a more urban pattern closer to the historic city center. In general, smaller setbacks infer a more urban condition while larger setbacks resemble a more rural condition. Implementation of setbacks along the corridors can be related to development zones which can guide the siting of the buildings from the road. The setbacks have been grouped into three conditions as described herein. General recommendations for building setbacks shall apply to all zones:

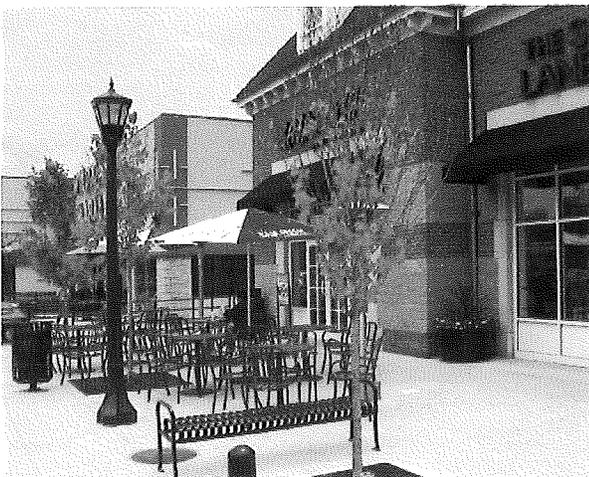
- (1) All buildings are to be placed as to address the road frontage. Buildings that are placed to the rear or middle of a site are not permitted. Large format retail centers may be placed to the rear of a site only if the development utilizes outlots to better address the street front.
- (2) Off-street parking or paved circulation areas shall not be placed forward of the building façade that has the road frontage. Parking fields must be placed to the side and rear of the structure. Pavement setbacks must be at least 5 to 10 feet behind the front of the structure.
- (3) Corner lots are considered to have a front setback along each public right-of-way. The building's design, architecture, and massing should emphasize the importance of the corner and draw attention to both sides of the streets. This will help

pedestrian and vehicular traffic “turn the corner” and emphasize the availability of site access secondary side streets or to the rear of the site. Parking areas must not be permitted on street corners.

(4) Adequate sideyards must be maintained to meet landscaping and screening recommendations. A minimum pavement and building setback of 25 feet from the side property line shall be provided between conflicting zoning districts (i.e. B-2 and R-3). This setback can be reduced if additional screening beyond the recommended landscape requirements is provided.

(5) Ground-mounted freestanding signs shall have a setback of at least 8 feet from the right-of-way. This setback can decrease if the sign base is integrated with landscaping for a corridor streetscape design.

**PEDESTRIAN ZONE AND STREETScape**



*Pedestrian Zone - should feature engaging elements such as outside dining and patio areas. These should be accessible from the street front.*

All parcels fronting onto a public street will have a “pedestrian zone”. This zone occurs between the curb and the front of development whether it be a building, pavement, or landscaping. This applies not only to the primary corridor road but also the intersecting public roads.

(6) This zone shall be designed for the pedestrian and well landscaped/hardscaped. The pedestrian zone should be designed to make it more comfortable for pedestrian or bicycle movements along the corridor roads.

(7) This zone also contributes greatly in providing for a well planned and executed gateway corridor into the more historic portions of downtown Urbana. Opportunities exist to create a unique streetscape for the area that is coordinated and aesthetically pleasing. The pedestrian zone and streetscape should be designed in accordance with the appropriate Corridor Study.

**DEVELOPMENT SUBAREA A - GENERAL URBAN STANDARDS**

The General Urban development pattern are portions of the corridor that are adjacent to a historic residential or commercial district. They can be characterized by an extension of a grided street system from the urban edge of historic business or residential areas of Urbana, a rear alley system and shallower development parcels.

(8) A similar build-to line shall be followed from the historic portions into the corridor area. Buildings shall have a minimum setback of 30 feet and a maximum of 40 feet from the curb of the primary road.

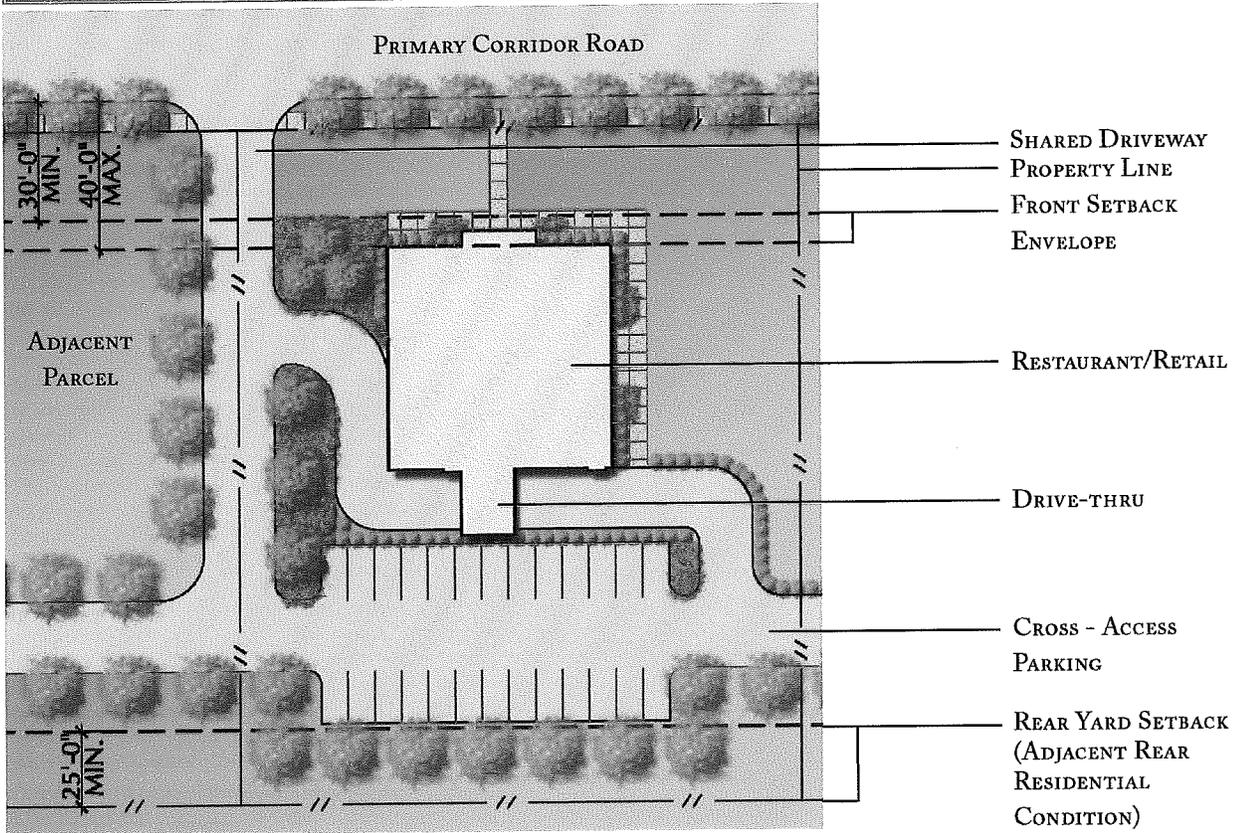
(9) Minimum parking setbacks shall be 35 feet from the curb or 5 feet behind the primary facade of the building.

(10) Ideally, setbacks should provide for a 4 foot tree lawn, a four to six foot concrete sidewalk, and 20 feet between the sidewalk and the building face for pedestrian amenities such as landscaped areas, pocket parks, or outdoor dining spaces.

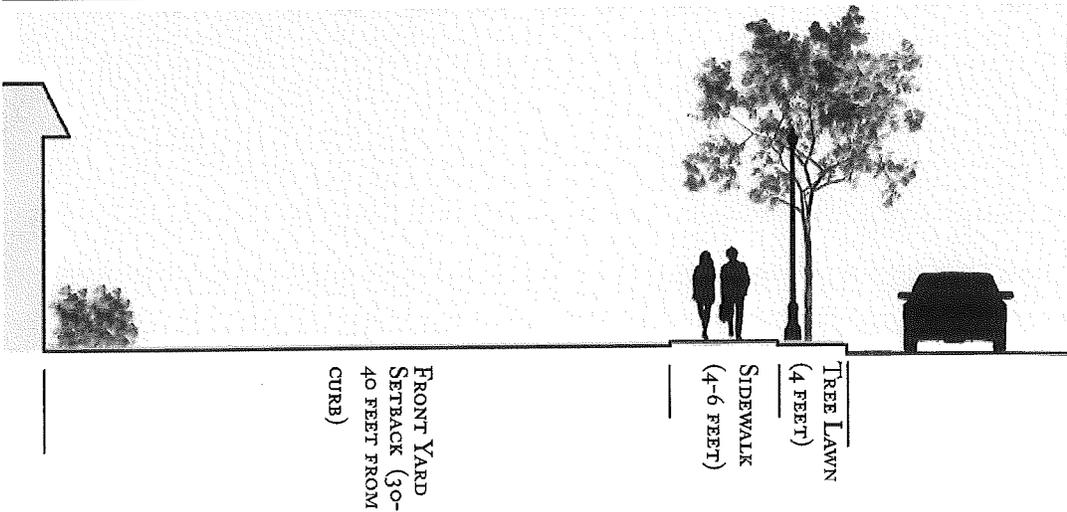
(11) A 5 foot rear yard pavement setback shall be required for parcels adjacent to a public alley. No rear pavement setback is required for parcels containing a rear cross access with adjacent parcels.

(12) For commercial lots abutting residential uses in the side or the rear yard, the setback shall be 25 feet. For adjacent commercial uses, building and parking lot setbacks must provide for all perimeter landscaping, parking lot screening, and site landscaping requirements.

DEVELOPMENT SUBAREA A - GENERAL URBAN  
SITE EXAMPLE



DEVELOPMENT SUBAREA A - GENERAL URBAN  
PRIMARY CORRIDOR ROAD STREET SECTION



**DEVELOPMENT SUBAREA B - TRANSITIONAL URBAN  
STANDARDS**

The Transitional Urban development pattern provides a mediary condition between the General Urban and the Rural Reserve. Setbacks in this zone increase but are not representative of a rural development pattern with full setbacks. Alleys are usually not present in this zone and the depth of parcels generally increase. Pavement must be placed to the rear or side of the building. A large green corridor should be established between the curb of the primary road and the building front.

(12) Buildings shall have a minimum setback of 50 feet and a maximum of 60 feet from the curb of the primary road.

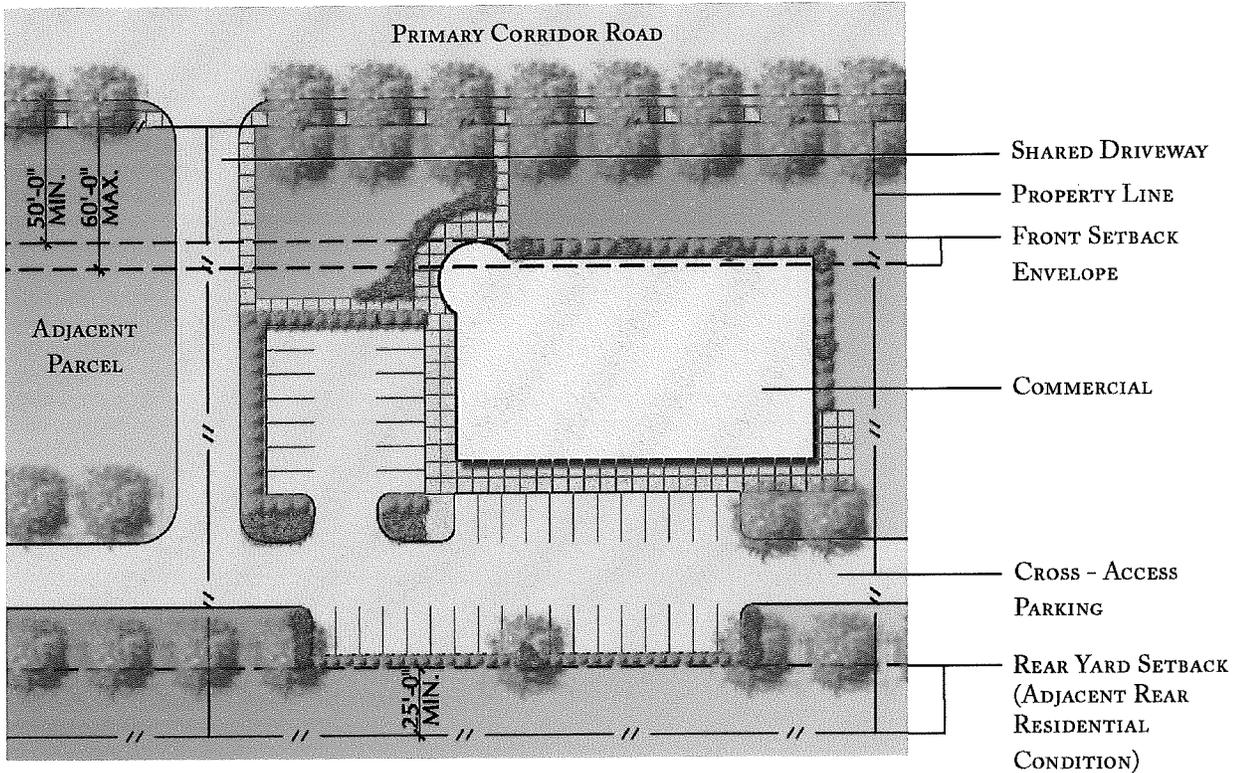
(13) Minimum parking setbacks shall be 55 feet from the curb or 5 feet behind the primary facade of the building.

(14) An ideal 6' tree lawn should be present along with a 6' sidewalk or bikepath. Paths shall be of concrete paving.

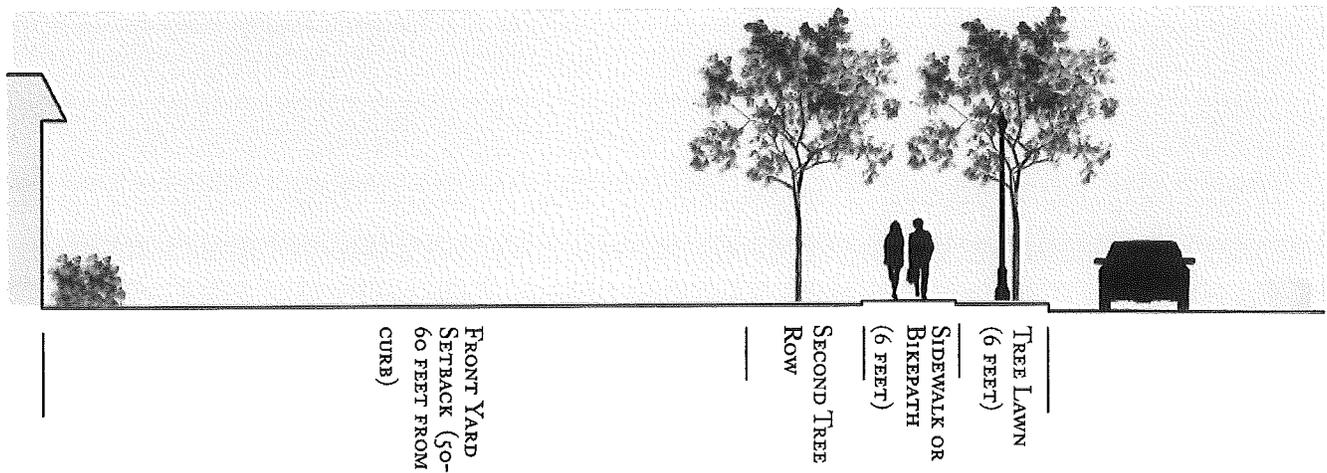
(15) A continual green corridor of at least 35 feet in width should be established along the primary road between the sidewalk and the front building facade.

(16) For commercial lots abutting residential uses in the side or the rear yard, the setback shall be 25 feet. For adjacent commercial uses, building and parking lot setbacks must provide for all perimeter landscaping, parking lot screening, and site landscaping requirements.

DEVELOPMENT SUBAREA B - TRANSITIONAL  
SITE EXAMPLE



DEVELOPMENT SUBAREA B - TRANSITIONAL  
PRIMARY CORRIDOR ROAD STREET SECTION



DEVELOPMENT SUBAREA C - CITY EDGE  
STANDARDS

The Rural Reserve development subarea serves as a transition for development to the more rural nature of many of the corridor roads leading away from Urbana. Many existing buildings along these roads have deep building setbacks and are situated on large parcels. This development zone allows for large format types of development and subsequently be placed in this subarea.

(17) Buildings shall have a minimum setback of 60 feet and a maximum of 70 feet from the curb of the primary road.

(18) An ideal 6' tree lawn should be present along with a 6' sidewalk or bikepath for the primary corridor road. The paths shall be concrete paving.

(19) Large format buildings shall be located closer to streets to minimize the scale of the overall development and reduce the expansiveness of parking areas located forward of a building. Outlots can also be created to break up the sight line between the corridor road and the large format retail building.

(20) The maximum setback for large format buildings shall be 400 feet. Large format buildings may not be located at the maximum setback and contain uninterrupted parking areas between the primary road the building facade. Outlot buildings must be used to break up these parking areas and create a development front along the corridor road.

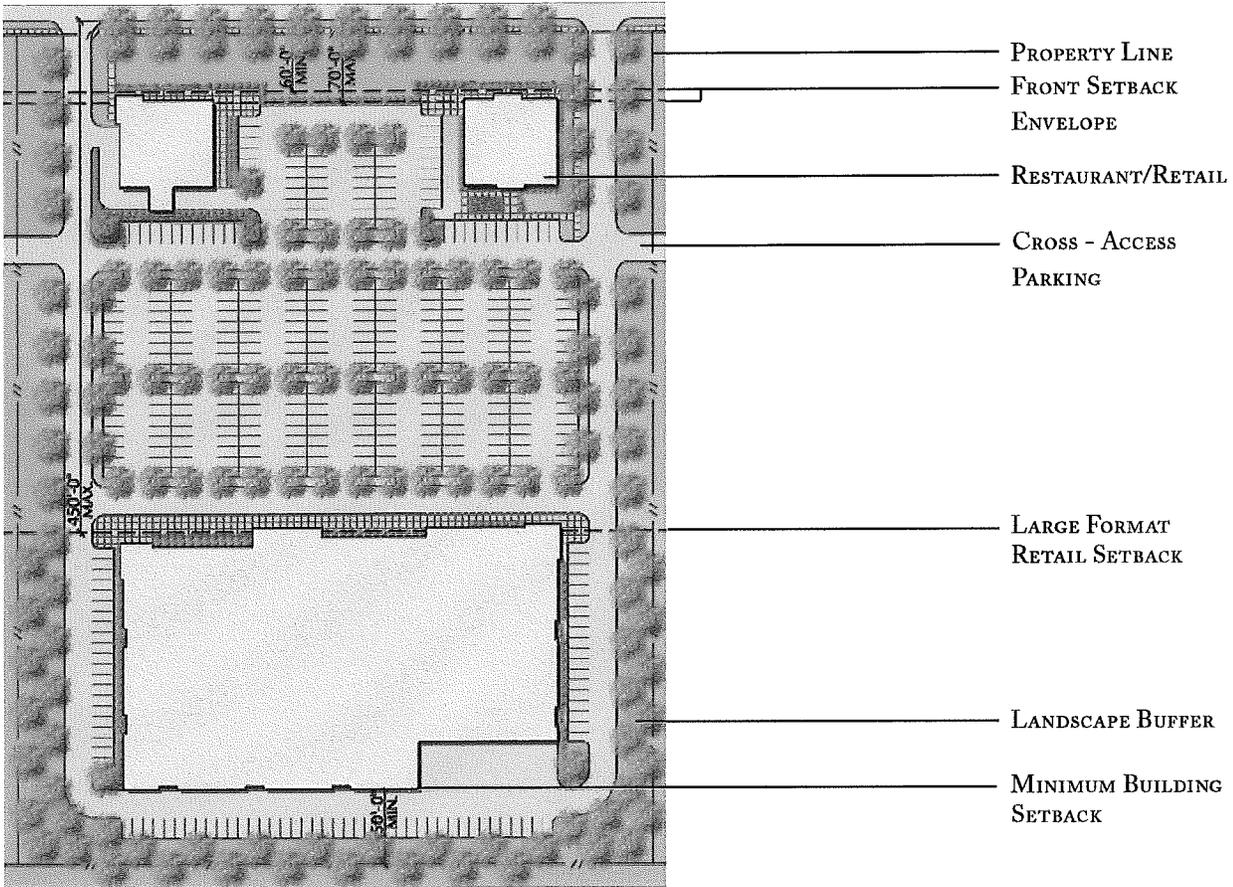
(21) Minimum parking setbacks shall be 65 feet or 5 feet behind the primary facade of the building (except for large format buildings where outlots are utilized).

(22) It is preferred that no more than 50 percent of the required off-street parking spaces for the store be located between the main building façade and the abutting street(s).

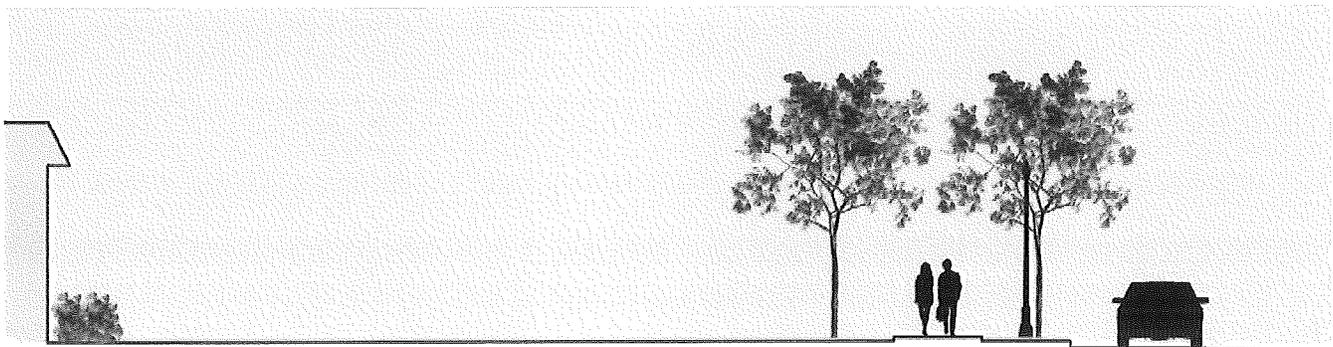
(23) For commercial lots abutting residential uses in the side or the rear yard, the building and parking lot setback shall be 25 feet. For adjacent commercial uses, building and parking lot setbacks must provide for all perimeter landscaping, parking lot screening, and site landscaping requirements.

(24) Large format buildings shall be located no closer than 50 feet to any side or rear lot line

DEVELOPMENT SUBAREA C - CITY EDGE  
SITE EXAMPLE



DEVELOPMENT SUBAREA C - CITY EDGE  
PRIMARY CORRIDOR STREET SECTION



OUTLOT  
FRONT YARD  
SETBACK  
(60-70 FEET  
FROM CURB)

SECOND TREE  
ROW  
SIDEWALK OR  
BIKEPATH  
(6 FEET)  
TREE LAWN  
(6 FEET)

A. SITE DEVELOPMENT

### LANDSCAPING AND SCREENING

Landscaping is necessary to provide for screening, streetscape improvements, and to enhance the overall aesthetics of a site. Overall, landscaping should be well executed in order to accentuate, but not over dominate the built environment.

(25) Street trees are required along all public road frontages within the corridor. Where street trees in the private site property are missing, dead, or dying the property owner is responsible for replacing them. The city will maintain all street trees within the public right-of-way. Street trees must meet city standards for species and spacing as determined in Chapter 905 (Trees) of the codified ordinances.

(26) In order to break up large areas of impervious surfaces, provide heat abatement, and enhance the appearance of the community, interior landscaping requirements are necessary. Any parking areas of 20 or more vehicular parking spaces shall provide interior landscaping. At least one shade tree shall be provided for every 20 vehicular parking spaces.

(27) Interior landscaping may be peninsular or island types, and must include at least one deciduous shade tree per peninsula or island. The minimum area for an internal landscape island is 20 square feet. All internal landscape islands must maintain a four foot clear zone from the base of the tree. Landscape islands must be fully curbed in order to protect the plantings. The remaining area of the landscape island must be filled with grass and mulch, shrubs, or ground cover. Mulching of the entire area shall not be permitted.

(28) A perimeter landscape buffer shall provide screening for vehicle parking areas and a buffer between conflicting land uses. It shall also be used to reduce the effects of glare from automobile headlights and to screen the appearance of parking areas from public rights-of-way and adjacent properties. A buffer of four to ten feet



*Landscape Island - provided in parking lots for every 20 vehicular parking spaces*



*Screening - parking areas must be screened from views from the public right-of-way*

around parking areas shall be established for landscape screening. Landscape screening of parking areas shall be at least a minimum of 3.5 feet high but have a mature growth at not more than 5 feet.

(29) The perimeter landscape buffer used for parking lot screening shall consist primarily of evergreen planting materials with a mix of deciduous and evergreen trees. Low stone walls, wrought iron fencing, and earth mounds can be used in conjunction with the landscaping.

(30) Perimeter landscaping used as a buffer between conflicting uses must be added where setbacks for such are required. A landscaped buffer between 6 and 12 feet must be established. This buffer shall consist of a mix of evergreen and deciduous shrubs and trees. Plantings shall be arranged as to be natural in appearance and to mimic established tree rows. See referenced perimeter landscape buffer in Appendix C.

(31) Perimeter landscaping located adjacent to a corridor road shall coordinate with the standards established by the City of Urbana corridor streetscape plan. This would include standards set for the type of landscaping materials to be used, species and location of street trees, any specialty treatments such as fencing, walls, lighting, and signage requirements.

(32) It is encouraged that efforts be made to preserve natural vegetation areas. Consideration shall be given to laying out streets, lots, structures, and parking areas to avoid the unnecessary destruction of heavily wooded areas or outstanding tree specimens. It is further encouraged that, whenever possible, heavily wooded areas be designated as park reserves.

(33) Site landscaping, shall be used to enhance the appearance and customer attraction of commercial areas, the architectural character and aesthetics



*Screening – combination of stone wall and landscaping for an effective screening*



*Site landscaping – located around buildings and equal to at least 2% of building ground coverage*

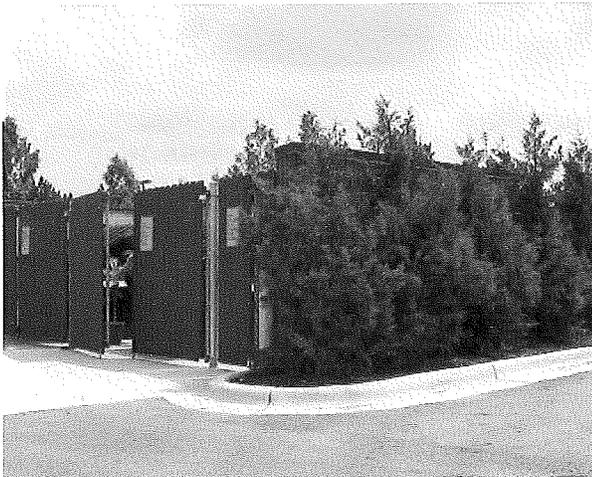
of the site, and the beautification of the city. Additional landscape material should be located around the building and other portions of the site. This landscaping does not include material that has been provided for interior parking lot landscaping, perimeter landscaping, or street trees. Recommended site landscaping shall be equal to two percent of the building ground coverage area.

(34) Unless otherwise specified, minimum size of all plant material at installation shall be two-inch caliper for deciduous shade trees, six feet high for evergreen trees, and two-inch caliper for ornamental trees/shrubs. If shrubs are to be installed for screening requirements, they shall be installed at three feet in height and meet full screening requirements within four years after planting.

LANDSCAPING  
DEVELOPMENT SUBAREA C - CITY EDGE

(35) Preserve areas of open space for active and passive recreation (Must provide open/park space per PUD Regulations).

(36) There shall be green space margins at least 20 feet wide around the periphery of the site. All parking areas shall be screened from the public right-of-way and residential districts. Screening may utilize a landscaped mound or evergreen shrub. Landscape mounds shall be planted with a mix of evergreen and deciduous tree and shrub species. Plantings should be arranged as to be natural in appearance and to mimic established tree rows. [See referenced landscape buffer in Appendix C.]



*Dumpster Screening - all dumpster and storage areas must be screened on all sides with a 100% opaque wall or gate that extends one foot above the item enclosed*

**OUTDOOR DISPLAYS & SEASONAL SALES**

(37) Outdoor display related to the principal business of any type, except for seasonal sales, is not permitted unless fully screened from view by 100% opaque screening. Outdoor display areas shall be located adjacent to the primary structure and not located more than 25 feet away. Stacking of materials within outdoor storage areas shall not exceed 6 feet in height.

(38) Outdoor displays of primary products associated with a business in which the business solely relies on the sale of these products (i.e. cars at automobile dealers, tractors at a farm equipment dealer) shall be permitted in the district. Products must be attractively displayed and not intrude upon the landscape. Urbana city staff and the Planning Commission must review and approve the location and nature in which the products are displayed. Any new or substantially redeveloped sites to include this type of sales are subject to this review.

(39) Stand-alone seasonal sale businesses such as farmer's markets, flowers, landscaping, and christmas tree sales are permitted and shall be regulated by the zoning code.

(40) Outdoor displays of vending or ice machines are not permitted and must be fully enclosed.

**SERVICE/STORAGE AREAS/DUMPSTERS**

(41) All service areas, such as dumpsters and loading areas shall be confined to the back of the principal structure or lot.

(42) Loading and refuse areas shall be consolidated, shared where possible, and attractively and completely screened.

(43) All storage areas (for machinery equipment, dumpsters, trash recyclables, etc.) shall be screened on all sides (except gated access) with 100% opaque wall that extends one foot above the top of any

container enclosed. The accessible side may use a gate, provided it also extends one foot above the top of any container and is not made of chain link or vinyl.

(44) The materials and colors selected for the enclosure's screening shall be identical to those on the principal building's exterior.

#### SITE LIGHTING

(45) Parking lots should be lighted. This lighting shall be appropriate to illuminate parking and building entrances with minimal glare or light trespass on neighboring properties or public roadways. A maximum of one-foot candle is permissible at all property lines. A photometric plan is required to show proposed light intensity.

(46) Parking lots can be illuminated by pole-mounted fixtures or by building-mounted fixtures. All light sources must be concealed (100% cutoff). A maximum pole height shall be established for each Development Subarea as follows:

Development Subarea A - General Urban  
Maximum Height: 25 feet

Development Subarea B - Transitional Urban  
Maximum Height: 25 feet

Development Subarea C - City Edge  
Maximum Height: 30 feet

(47) All light poles shall be in curb protected landscape islands. Concrete footers to mount light poles should not extend more than six inches above grade.

(48) Decorative landscape lighting and accent building lighting is permitted, but should not impair the safety of public roadways or the enjoyment of neighboring properties. Uplighting must be from concealed sources. If the lighting is mounted on the ground, evergreen planting or shields shall be used to fully screen and hide light sources.



*Site Lighting - poles must be placed in curbed landscape islands with footers extending no more than six inches above the ground*

**Table 1.**

MINIMUM VEHICLE STACKING SPACES (PER WINDOW/BAY)	
Use	Spaces
Drive-thru restaurant	8
Drive-thru convenience store	8
Car wash (all types)	4
Drive-thru bank	4
Automated teller machine	4
Oil changing facilities	4
Gas station	2

**AUTO-ORIENTED USES**

(49) Auto-oriented are those facilities where a service is rendered or a sales transaction is made while the patron is typically not required to exit his/her vehicle, or a facility that includes services rendered directly on, to or for vehicles.

Auto-oriented commercial facilities include, but are not limited to drive-thru restaurants, drive-in restaurants, automated teller machines (ATMs), drive-thru banks, car washes (all types), gas stations, facilities specializing in oil changes, car repair, establishments, installing car accessories, and stand-alone parking lots. The sale of vehicles (new or used) is not included as an auto-oriented use.

(50) Auto-oriented businesses are to be considered a conditional use and can be allowed in the corridor district after a permit is granted by the Building and Zoning Appeals Board. Development standards for auto-oriented uses within these standards shall be met along with the general standards for conditional uses as required by the Zoning code in Section 1105.25 through 1105.32.

(51) Service bays and garage door openings should not front onto a public road. If service bays are to be located adjacent to a public road, they must be screened from view by evergreen landscaping. Service bays must be closed off completely with a opaque door that is decorative and contains relief. Doors may not be completely opaque

(52) All directional signage associated with drive-thru lanes, teller identification, etc. must follow the directional sign standards (see Directional Signs, D(61)-D(65)).



*Service Bays- should not front onto a public road and screened from view*

(53) Some auto-oriented uses are associated with facilities that can produce excessive noise that may become a nuisance to neighboring parcels. This would include but not limited to such uses as automatic car washes, vacuum cleaners, and auto-repair facilities. These uses can use screening walls or evergreen landscape material as noise control elements. A limited time period shall be placed on these uses and limit their operations to 7 a.m. to 10 p.m.

(54) Drive-thru canopies shall be located to the side or rear of the structure. The appearance of the canopy shall be coordinated so that the size of the canopy is in proportion to the size of the columns supporting the canopy.

(55) Sufficient vehicle stacking shall occur for auto-oriented uses. All auto-oriented uses must provide at least four vehicle stacking spaces unless otherwise noted in the Table 1. Stacking of the vehicles however, must not occur between the front building façade and the road.

(56) No curb cuts larger than 30 feet in width. All curb cuts must have at least a 6 inch concrete curb to protect the tree lawn.

(57) There should be a pass-through lane available for drive-up service lanes.

#### UTILITIES

(58) Permanent utility lines shall be installed underground or relocated to the rear of the parcel.

(59) Streetscape sidewalk, landscaping, and other areas in the pedestrian zone shall be restored to original condition (or per design standard) immediately following any necessary utility cut disturbing the ground.



*Drive-thrus - must be located to the rear or side of a building.*

