

**URBANA CITY COUNCIL
REGULAR SESSION MEETING AGENDA
TUESDAY, FEBRUARY 19, 2019, at 6:00 P.M.**

(The UCC Regular Session Meeting will be held in Council Chambers/the Municipal Building)

Call to Order

Roll Call

Pledge of Allegiance

Approval of Minutes

Urbana City Council Regular Session meeting minutes of February 5, 2019.

Communications

1. Champaign Countywide Public Safety Communications Center Financial Report – 2018 (See attached)
2. City of Urbana, Ohio 2018 Curbside Recycling Report (See attached)
3. Letter from Board of Elections regarding ballot language (See attached)
4. Letter Champaign Economic Partnership (See attached)
5. South High Street Corridor Improvements Study (See attached)

Board of Control

1. The Board of Control recommends that Council authorize the Director of Administration to enter into Scope of Work No. 3 (SOW3) under the Master Services Agreement with Utility Service Co., Inc. of Atlanta, Georgia for the long term care and maintenance of the East Elevated Water Tank (Tower) located at 1350 Scioto Street. In contract years 1 - 5, the cost shall be \$85,432.00 per year. In contract year 6, the annual fee shall be \$45,177.00. After contract year 6, the contract shall be subject to an adjustment of up to 5% per annum. Utility Service Co., Inc. was selected through a request for proposal process in early 2014. This yearly expense will be paid from the Water Fund, and this expense is in the 2019 budget.

VOTE: 3-0

2. The Board of Control recommends that Council authorize the Director of Administration to enter into Scope of Work No. 4 (SOW4) under the Master Services Agreement with Utility Service Co., Inc. of Atlanta, Georgia for the long term care and maintenance of the West Elevated Water Tank (Tower) located at 726 S. Edgewood Avenue. In contract year 1 the cost shall be \$2,500.00. In contract years 2 - 6, the annual fee shall be \$85,339.00. In contract year 7, the annual fee shall be \$46,776.00. After contract year 7, the contract shall be subject to an adjustment of up to 5% per annum. Utility Service Co., Inc. was selected through a request for proposal process in early 2014. This yearly expense will be paid from the Water Fund, and this expense is in the 2019 budget. VOTE: 3-0

3. The Board of Control and Health Insurance Committee recommends Council authorize the purchase of UnitedHealthcare group health insurance and related life, vision (EyeMed), and dental (Delta Dental) insurances for \$1,626,367.00. The insurance expense is in the 2019 budget at \$1,594,876.00 (see attached). VOTE: 3-0

Footnote - \$2,501 -\$50,000 Purchase Order list for January 2019 (see attached)

Citizen Comments

Ordinances and Resolutions

Third Reading

ORDINANCE NO. 4385-19

AN ORDINANCE TO AMEND CHAPTER 141 OF THE URBANA, OHIO CODIFIED ORDINANCES CONCERNING AMBULANCE SERVICE RATES AND CHARGES. (Requires three readings)

Second Reading

ORDINANCE NO. 4397-19

AN ORDINANCE ESTABLISHING THE SALARY AND BENEFITS FOR THE FIRE CHIEF OF THE CITY OF URBANA. (Required three readings)

ORDINANCE NO. 4398-19

AN ORDINANCE ESTABLISHING THE SALARY AND BENEFITS FOR THE ASSISTANT FIRE CHIEF OF THE CITY OF URBANA. (Requires three readings)

ORDINANCE NO. 4399-19

AN ORDINANCE ESTABLISHING THE SALARY AND BENEFITS FOR THE CENTRAL STAFF EMPLOYEES OF THE CITY OF URBANA. (Requires three readings)

ORDINANCE NO. 4400-19

AN ORDINANCE ESTABLISHING THE SALARY AND BENEFITS FOR THE POLICE CHIEF OF THE CITY OF URBANA. (Requires three readings)

ORDINANCE NO. 4401-19

AN ORDINANCE ESTABLISHING THE SALARY AND BENEFITS FOR THE POLICE LIEUTENANT OF THE CITY OF URBANA. (Requires three readings)

ORDINANCE NO. 4405-19

AN ORDINANCE FIXING SALARY AND OTHER BENEFITS FOR THE DIRECTOR OF ADMINISTRATION. (Requires three readings)

ORDINANCE NO. 4406-19

AN ORDINANCE FIXING SALARY AND OTHER BENEFITS FOR THE DIRECTOR OF FINANCE. (Requires three readings)

First Reading

RESOLUTION NO. 2546-19

A RESOLUTION ENACTED BY THE CITY OF URBANA, CHAMPAIGN COUNTY, OHIO, TO AUTHORIZE THE DIRECTOR OF ADMINISTRATION TO SIGN ALL DOCUMENTS REQUIRED FOR THE COMBINATION OF ODOT PID #100783 (URBAN PAVING PROGRAM) WITH ODOT PID #103793 (US 36/US 68 INTERSECTION IMPROVEMENT-MONUMENT SQUARE ROUNDABOUT), AND DECLARING AN EMERGENCY. (May be passed on 1st reading)

Miscellaneous Business

Adjourn

Work Session

**URBANA CITY COUNCIL
REGULAR SESSION MEETING
TUESDAY, FEBRUARY 5, 2019**

President Hess called the City of Urbana Regular Session Meeting to order at 6:00 p.m. City staff attending: Mayor Bill Bean, Director of Administration Kerry Brugger, Director of Finance Chris Boettcher, Director of Law Mark Feinstein, Community Development Director Doug Crabill, Water Reclamation Facility Director Chad Hal, and City Engineer Tyler Bumbalough.

PRESIDENT CALLED ROLL: Mr. Fields, present; Mr. Hoffman, present; Mr. Paul, absent; Mr. Scott, present; Mr. Thackery, absent; Mr. Pena, present; and Mr. Piper, present.

President Hess led those present in the Pledge of Allegiance.

MINUTES of the Regular Session Meeting Minutes of January 15, 2018, were presented for approval. Mr. Fields moved for the approval of said minutes and Mr. Pena seconded. Voice vote on approval: all ayes, nays, none.
Motion passed 5-0.

COMMUNICATIONS

1. Board of Zoning Appeals Meeting Minutes from 12-10-2018.
2. Ohio Ethics Commission Filing Reminder.
3. Letter from Champaign County Board of Elections.
4. Design Review Board Meeting Minutes from 12-17-18.
5. Planning Commission Meeting Minutes from 11-26-18.
6. Oak Dale Cemetery Board Meeting Minutes from November 15, 2018.
7. Park Board Meeting Notes from December 10, 2018.
8. City Annual Review.
9. Letter from Adam Moore, Zoning & Compliance Officer to Council regarding 861 S. Main St., Rezoning Application.

Mr. Hoffman moved to accept all of the communications. Mr. Fields seconded. Voice vote on approval: all ayes, nays, none.
Motion passed 5-0.

ADMINISTRATIVE REPORTS – BOARD OF CONTROL

1. **McGuire Farm & Excavating LLC of Urbana – for the 2019 Unit Price Concrete Work**
Mr. Bumbalough stated that it was great to have some competition this year. He also stated that this company came in lower than J&J Schlagle and has heard good things about this company. He also mentioned that there were a few issues with J&J Schlagle last year.

Mr. Pena moved to put this item on the floor for discussion and passage. Mr. Hoffman seconded. Voice vote on approval: all ayes, nays, none. Motion passed 5-0.

2. **J.M. Mason, Inc. - \$696,181.00 (OWDA Loan and debt payments will be included in subsequent budget years)**

Mr. Hall stated that J.R. Mason was the lowest bid. He also mentioned that a related company, R.J. Mason, Inc. did the 1975 upgrade and the 1990 enhancement job.

Mr. Hoffman moved to put this item on the floor for discussion and possible passage. Mr. Pena seconded. Voice vote on approval: all ayes; nays, none.

Motion passed 5-0.

CITIZEN COMMENTS

None

ORDINANCES AND RESOLUTIONS:

Third Reading –

ORDINANCE NO. 4426-19

AN ORDINANCE TO REVISE THE CODIFIED ORDINANCES BY ADOPTING CURRENT REPLACEMENT PAGES. (Requires three readings)

Mr. Hoffman moved to put this ordinance on the floor for discussion and possible passage. Mr. Fields seconded. President Hess called for a roll call for passage: Mr. Hoffman, yes; Mr. Scott, yes; Mr. Pena, yes; Mr. Piper, yes; and Mr. Fields, yes.

Ordinance passes 5-0.

Second Reading –

ORDINANCE NO. 4385-19

AN ORDINANCE TO AMEND CHAPTER 141 OF THE URBANA, OHIO CODIFIED ORDINANCES CONCERNING AMBULANCE SERVICE RATES AND CHARGES.

Mrs. Boettcher stated that the billing rates will mirror what medicare and Medicaid will pay.

President Hess declared this ordinance to have its second reading.

First Reading –

ORDINANCE NO. 4397-19

AN ORDINANCE ESTABLISHING THE SALARY AND BENEFITS FOR THE FIRE CHIEF OF THE CITY OF URBANA.

Mr. Brugger advised that the last time this and the following ordinance were addressed was in 2013, and at that time the only thing changed was the expiration date, for that was removed. Mr. Brugger gave a summary of the changes.

President Hess declared this ordinance to have its first reading.

ORDINANCE NO. 4398-19

AN ORDINANCE ESTABLISHING THE SALRY AND BENEFITS FOR THE ASSISTANT FIRE CHIEF OF THE CITY OF URBANA.

Mr. Brugger gave a summary of the changes.

President Hess declared this ordinance to have its first reading.

ORDINANCE NO. 4399-19

AN ORDINANCE ESTABLISHING THE SALRY AND BENEFITS FOR THE CENTRAL STAFF EMPLOYEES OF THE CITY OF URBANA.

Mr. Brugger gave a summary of the changes as well as stated that the City was looking into job description process to match the pay with the responsibilities.

President Hess declared this ordinance to have its first reading.

ORDINANCE NO. 4400-19

AN ORDINANCE ESTABLISHING THE SALRY AND BENEFITS FOR THE POLICE CHIEF OF THE CITY OF URBANA.

Mr. Brugger gave a summary of the changes.

President Hess declared this ordinance to have its first reading.

ORDINANCE NO. 4401-19

AN ORDINANCE ESTABLISHING THE SALRY AND BENEFITS FOR THE POLICE LIEUTENANT OF THE CITY OF URBANA.

Mr. Brugger gave a summary of the changes.

President Hess declared this ordinance to have its first reading.

ORDINANCE NO. 4405-19

AN ORDINANCE FIXING SALARY AND OTHER BENEFITS FOR THE DIRECTOR OF ADMINISTRATION.

Mr. Brugger gave a summary of the changes.

President Hess declared this ordinance to have its first reading.

ORDINANCE NO. 4406-19

AN ORDINANCE FIXING SALARY AND OTHER BENEFITS FOR THE DIRECTOR OF FINANCE.

Mr. Brugger gave a summary of the changes.

President Hess declared this ordinance to have its first reading.

ORDINANCE NO. 4407-19

AN ORDINANCE FIXING SALARY AND OTHER BENEFITS FOR THE DIRECTOR OF LAW.

Mr. Brugger gave a summary of the changes.

Mayor Bean stated that changes needed to be made to state that the ranges had to be changed on all of these due to the fact that the lower range pay had to be brought up with the top out pay. Mr. Feinstein gave explanation that this was to make it more standard throughout the process.

President Hess declared this ordinance to have its first reading.

RESOLUTION NO. 2544-19

A RESOLUTION AUTHORIZING THE DIRECTOR OF ADMINISTRATION TO APPLY FOR, ACCEPT AND ENTER INTO A WATER SUPPLY REVOLVING LOAN ACCOUNT ON BEHALF OF THE CITY OF URBANA, OHIO FOR THE CONSTRUCTION OF THE PHASE 2 WATER MAIN REPLACEMENT PROGRAM AND DESIGNATING A REPAYMENT SOURCE FOR THE LOAN, AND DECLARING AN EMERGENCY.

Mr. Crabill spoke of the Phase I project which began in 2014 and was completed in 2015. He also added that this would start the process to try to get the funding.

President Hess called for a roll call for passage: Mr. Scott, yes; Mr. Pena, yes; Mr. Piper, yes; Mr. Fields, yes; and Mr. Hoffman, yes.

Resolution passed 5-0.

RESOLUTION NO 2545-19

A RESOLUTION TO DEDICATE THE PUBLIC SANITARY SEWER MAIN, TURN LAND IMPROVEMENTS, TRAFFIC SIGNAL, FLASHING SCHOOL ZONE SIGNAGE, AND RELATED ROADWAY SIGNAGE THAT WAS CONSTRUCTED AND INSTALLED BY THE URBANA CITY SCHOOL DISTRICT AND THE OHIO FACILITIES CONSTRUCTION COMMISSION UNDER A MEMORANDUM OF UNDERSTANDING WITH THE CITY OF URBANA, OHIO, AS PART OF THE CONSTRUCTION OF THE NEW PREK-8 SCHOOL LOCATED AT 1673 US ROUTE 68 SOUTH, AND DECLARING AN EMERGENCY.

Mr. Crabill said this was to allow the City to accept ownership of the items.

President Hess called for a roll call for passage: Mr. Pena, yes; Mr. Piper, yes; Mr. Fields, yes; Mr. Hoffman, yes; and Mr. Scott, yes.

Resolution passed 5-0.

MISCELLANEOUS BUSINESS/WORK SESSION

Mr. Fields moved to excuse Councilman Paul and Councilman Thackery. Mr. Pena seconded. Voice vote on approval: all ayes, nays, none.

Motion passed 5-0.

Mr. Hoffman stated that the City could make a lot of money off of people who speed in the school zone.

Mr. Bumbalough spoke of three projects coming up. He spoke about the Round-A-Bout, Phase III of the N. Oakland St. curbs and sidewalks project and the Gwynne St. Bridge Project. He said that pertaining to the Gwynne St. Bridge Project, the completion date of the same should be around March 31, 2019. He also added that they will be allowed a two week closure of the bridge to do the project.

Mr. Hall spoke to a sewer line replacement project involving Locust St., E. Ward and Washington Ave. and during that project the street will be down to one lane.

Mr. Feinstein spoke of Ordinance No. 3461, regarding Box 13. He recommended that this ordinance be rescinded due to the fact that the City cannot indemnify.

Mr. Fields moved to rescind Ordinance No. 3461. Mr. Pena seconded. President Hess called for a roll call to rescind this ordinance: Mr. Pena, yes; Mr. Piper, yes; Mr. Fields, yes; Mr. Hoffman, yes; and Mr. Scott, yes.

Motion passed 5-0.

Mr. Brugger stated the Street Department did a great job on snow removal and also advised the City should make it through the rest of the winter on the amount of salt the City presently has. He also spoke about a lot of discussion regarding communication. He stated that anyone can go to urbanaohio.com, go to the Facebook page or the website itself and look around. He added that there was a lot of information out there. He continued with the subject of 2018 In Review. He said that everyone should be pleasantly surprised with this. He also stated that per the Springfield newspaper, the Local Government Fund will not be coming back to the municipalities anytime soon. He also mentioned that he is requesting a Work Session on the 4th Tuesday of this month.

Mayor Bean said that he had seen Governor DeWine recently and it doesn't look like any funds would be coming back soon.

President Hess spoke about looking Councilman Pena, but he would be here through the month of March.

Mr. Fields moved to adjourn. Mr. Pena seconded. Voice vote on approval: all ayes; nays, none.
Motion passed 5-0.

ADJOURN AT 7:16 p.m.

NEXT SCHEDULED MEETING

February 19, 2019, at 6:00 p.m.

Council Clerk

Council President

**CHAMPAIGN COUNTYWIDE PUBLIC SAFETY COMMUNICATIONS CENTER
COUNCIL OF GOVERNMENTS
FINANCIAL REPORT
FUND 956
2018**

EXPENDITURES

ACCOUNT #	ACCOUNT NAME	ACTUAL EXPENDITURE
956-0956-001.01	Salary - Full Time Dispatcher	295,139.62
956-0956-001.02	Salary - Part Time Dispatcher	64,520.44
956-0956-001.03	Salary - Communications Manager	56,880.20
956-0956-001.04	Salaries - Overtime	39,376.14
956-0956-001.06	Salary - Assistant Director	46,622.40
956-0956-001.10	Sick Leave/Vacation Payoff	1,845.00
956-0956-002.00	COG Share Group Insurance	152,156.00
956-0956-002.01	COG Share Medicare	6,650.40
956-0956-002.02	COG Share PERS	70,450.94
956-0956-002.04	Unemployment	410.76
956-0956-002.05	Workers' Compensation	5,597.50
956-0956-003.00	Supplies - Office	3,106.34
956-0956-003.02	Supplies - Postage	20.00
956-0956-003.16	Supplies - Books/Periodicals	150.00
956-0956-004.00	Equipment - Office	2,718.56
956-0956-004.03	Equipment - Radio	3,487.96
956-0956-004.04	Equipment - Computer	230,478.55
956-0956-004.05	Equipment - Furniture	246.30
956-0956-004.06	Equipment - Uniforms	341.80
956-0956-006.10	Professional Services	11,558.91
956-0956-006.11	Services - Maintenance Contracts	27,326.16
956-0956-007.10	Building Lease/Utilities/Maintenance	15,498.00
956-0956-007.11	Repair/Maintenance - Equipment	829.90
956-0956-007.12	Computer Repair/Maintenance	1,240.90
956-0956-008.03	Utilities	24,709.04
956-0956-008.04	Advertising	179.70
956-0956-008.06	Travel and Meetings	480.72
956-0956-008.07	Mileage	936.21
956-0956-008.08	Training	1,509.91
956-0956-008.15	Audit Charges	-
956-0956-008.29	Insurance - Liability	15,502.00
956-0956-008.33	LEADS	7,200.00
956-0956-009.01	Dues/Memberships	284.00
956-0956-009.65	Fees - Settlement - Auditor/Treasurer	24,703.14
956-0956-009.66	Fees - Settlement	38.39
TOTAL EXPENDITURES		1,112,195.89

REVENUES

ACCOUNT #	ACCOUNT NAME	ACTUAL REVENUE
956-0001-001.00	Tax - General Real Estate	985,342.95
956-0001-002.00	Tax - Tangible Personal Property	-
956-0001-003.00	Tax - Manufactured Home	3,319.16
956-0001-011.00	Wireless Tax Revenue	89,103.55
956-0003-010.19	State Tax Revenues	94,293.73
956-0007-011.00	KTH Enterprise Zone	155.63
956-0010-001.00	Reimbursement	13,058.90
956-0010-004.00	Refund - Other	5,622.00
TOTAL REVENUES		1,190,895.92

Total Receipts Over/(Under) Disbursements	78,700.03
Fund Cash Balance, January 1	<u>1,633,105.78</u>
Fund Cash Balance, December 31:	
Assigned	-
Unassigned	<u>1,711,805.81</u>
Fund Cash Balance, December 31	<u><u>1,711,805.81</u></u>

City of Urbana, Ohio

2018 Curbside Recycling Report

	Recycle	Recycle	Recycle	Recycle	Recycle	Recycle	Change (Tons)	Change (%)
	Ttl (2018)	Ttl (2017)	Ttl (2016)	Ttl (2015)	Ttl (2014)	Ttl (2013)	(2018 vs. 2017)	(2018 vs. 2017)
Jan.	61.81	29.60	32.94	45.11	50.20	41.91	32.21	109%
Feb.	49.20	34.10	31.34	33.80	26.00	33.11	15.10	44%
Mar.	62.35	42.50	43.60	39.00	28.71	34.80	19.85	47%
Apr.	52.58	37.60	35.52	66.80	33.91	30.91	14.98	40%
May	68.30	30.30	35.25	38.60	49.80	43.52	38.00	125%
June	61.54	30.80	45.80	44.60	65.60	35.11	30.74	100%
July	53.02	30.90	37.20	43.60	73.71	49.91	22.12	72%
Aug.	60.46	31.30	26.31	33.52	62.52	53.71	29.16	93%
Sept.	47.31	46.61	61.80	33.20	51.31	41.91	0.70	2%
Oct.	53.30	48.45	31.99	38.31	48.91	46.40	4.85	10%
Nov.	63.76	63.83	38.03	29.20	54.71	38.52	-0.07	0%
Dec.	56.24	50.66	45.31	48.60	50.40	37.40	5.58	11%
	689.87	476.65	465.09	494.34	595.78	487.21	213.22	45%

2018 Program Highlights

45% increase in total tonnage of recyclables collected in 2018 vs. 2017

213.22 more tons of recyclables collected in 2018 vs. 2017

45% increase (year over year) in total tonnage since change to cart based collection in September 2017

Republican Members

ROGER L. TERRY
STEVE A. MOORE

Director

MEREDITH L. BODEY

Champaign County



Board of Elections

1512 S. US Highway 68, Ste L100
Urbana, Ohio, 43078
Phone: 937-484-1575 Fax: 937-484-1578

Democratic Members

KAREN F. WARD
Chairperson
LYNN R. MINTCHELL

Deputy Director

JODI L. FLORA

February 7, 2019

City of Urbana
Amy Deere, Clerk of Council
205 S. Main Street
Urbana, OH 43078

Dear Amy, Council members:

The enclosed language has been certified by the Board of Elections on Thursday, February 7th for the May 7th Primary Election.

If you have any questions or concerns please call, the Board of Elections at (937) 484-1575 as soon as possible. Thank you!

Sincerely,

A handwritten signature in cursive script that reads "Meredith".

Meredith L. Bodey
Director

OFFICIAL QUESTIONS AND ISSUES BALLOT

05/07/2019 PRIMARY SPECIAL

CHAMPAIGN COUNTY

PROPOSED MUNICIPAL INCOME TAX

**CITY OF URBANA
ORDINANCE #4486-19**

A majority affirmative vote is necessary for passage.

Shall the Ordinance providing for a 0.6% levy increase on earned income for the purpose of increasing funds solely dedicated to public safety (police/fire/ems) operating and capital improvements and sustaining these services effective July 1, 2019 be passed?

- For the Income Tax
- Against the Income Tax

THE UNIVERSITY OF CHICAGO
DEPARTMENT OF CHEMISTRY
LABORATORY OF ORGANIC CHEMISTRY

REPORT OF RESEARCH
BY
DR. [Name]
AND
MR. [Name]

Submitted in partial fulfillment of the requirements for the Ph.D. degree
to the University of Chicago, Chicago, Illinois

1954

"Working Together for Success"



February 7, 2019

City of Urbana
Attn: Chris Boettcher
205 S. Main St.
Urbana, OH 43078

Dear City of Urbana,

On behalf of the Champaign Economic Partnership Board and the staff, we would like to thank City of Urbana for the generous investment for 2019 and your continued support of our agency as the 5th year of the CEP. We appreciate your confidence as we continue working on your behalf for economic development in Champaign County.

Your support will assist the CEP to continue striving for the best in workforce, education and economic opportunities to grow our community.

Thank you for your support and we will be in touch soon,

A handwritten signature in cursive script that reads 'Marcia Bailey'.

Marcia Bailey

Director

"Working Together for Success"



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SOUTH HIGH STREET CORRIDOR IMPROVEMENTS FEASIBILITY STUDY
FINAL REPORT

Prepared for:
City of Urbana
Clark County-Springfield Transportation Coordinating Committee
February 2019

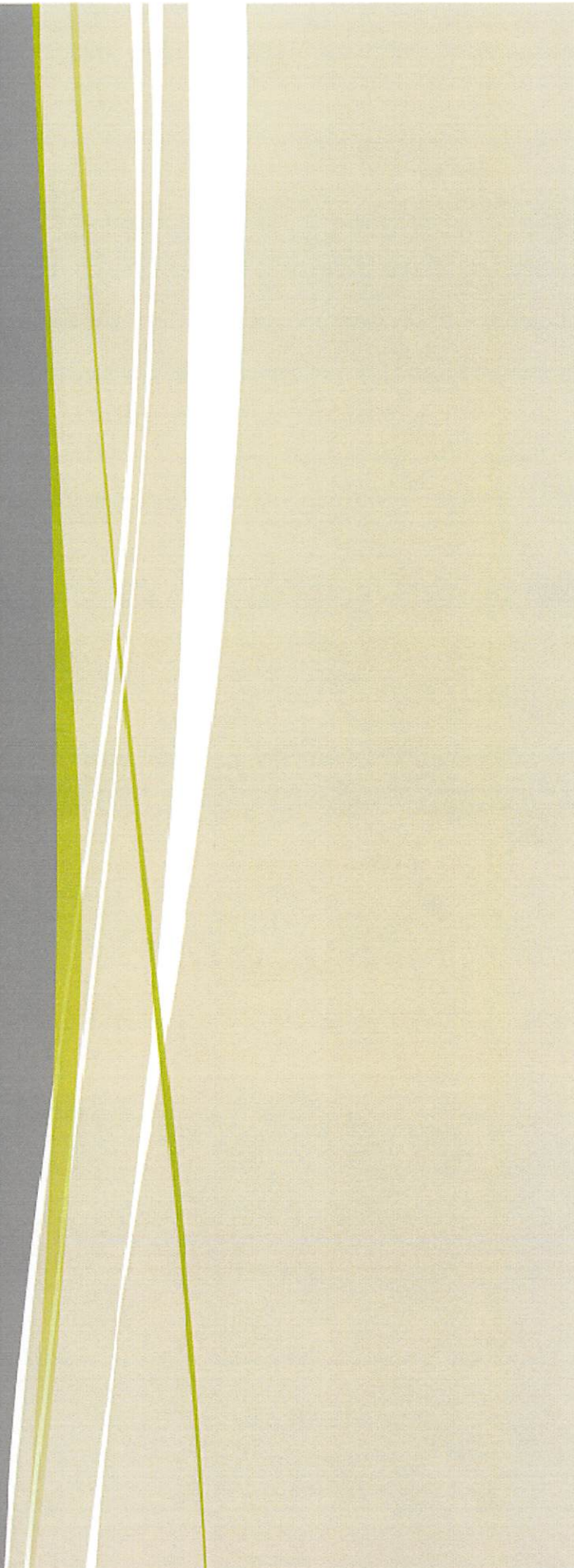


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Project Description

South High Street is an approximately mile long corridor near the southern limits of Urbana, intersecting Miami Street (U.S. 36) at the northern end and S.R. 55 (Lewis B. Moore Drive) at the southern end of the study limits. The City of Urbana, in cooperation with the Clark County-Springfield Transportation Coordinating Committee (CCSTCC), is exploring opportunities to improve sidewalk connectivity, provide bicycle infrastructure, incorporate on street parking and address drainage issues within the project corridor. The City anticipates using the feasibility study finding to pursue funding alternatives. The project location is illustrated in **FIGURE 1**.



FIGURE 1

FIGURE 1: PROJECT LOCATION

Using the on-call study contract with CCSTCC, B&N was tasked to provide recommendations for potential improvement within the project corridor to meet the project needs for the City of Urbana. The improvements addressed the project goals which included the following:

- Providing pedestrian and bicycle connectivity for residents and Urbana University Staff and students
- Address any safety concerns at Miami Street / High Street and S.R. 55 / High Street
- Identify other potential corridor improvements such as on-street parking, drainage upgrades, and traffic speeds
- Develop cost estimates for proposed funding applications

The project scope included performing a traffic analysis to determine the capacity of the corridor and identify any capacity related improvements. Additionally, the safety of the corridor was studied to identify any potential crash patterns and determine potential improvements to address safety concerns. Proposed typical sections were provided and a cost estimate prepared to assist with funding applications.

This report describes the corridor characteristics, traffic and safety study findings, public input, and potential improvements for the corridor.

Corridor Characteristics

The corridor being studied is High Street from the intersection of Miami Street to the intersection of S.R. 55. The corridor is illustrated in **FIGURE 1**.

High Street is a two-lane urban road. The roadway is classified as a *Minor Collector* with a posted speed of 25 mph. From College Street to Miami Street, the roadway typically has a 10' travel lane in each direction, curb, and a sidewalk with a width of 4' to 5' on both sides of the street. A minimal buffer is present adjacent to the walk in some locations. In other locations the walk is adjacent to the roadway. South of College Street, the existing roadway typical section changes to 10.5' to 11' lanes in each direction, no or intermittent curb, and no sidewalks. The City staff has observed pedestrians walking in the roadway in portions of the corridor without sidewalks. There is no designated parking within the project. residents currently pull off along the roadway on gravel, paved, or dirt shoulders. Existing lighting in the corridor is on utility poles owned by Dayton Power & Light / Miami Valley Lighting. The lighting is primarily at intersections and in between blocks. Right-of-way widths vary in the corridor.

TABLE 1 summarizes the widths from the southern project limits to the northern project limits.

FIGURE 2 and **FIGURE 3** illustrate the existing typical sections for the corridor.

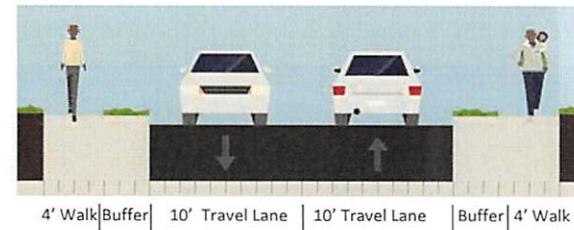


FIGURE 2: TYPICAL SECTION NORTH OF COLLEGE STREET

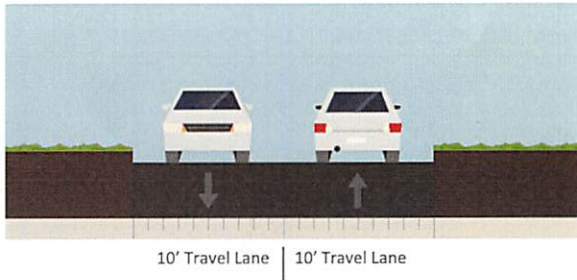


FIGURE 3: TYPICAL SECTION SOUTH OF COLLEGE STREET

TABLE 1: EXISTING RIGHT-OF-WAY

LOCATION	RIGHT-OF-WAY WIDTH
SR 55 to W Broadway Street	50'
W Broadway Street to Powell Avenue	60'
Powell Avenue to Thompson Street	50'
Thompson Street to College Street	45'
College Street to Miami Street	40'

Most of the side streets intersect South High with stop signs. Three four-way stops are present in the corridor at the intersections of Reynolds Street, Thompson Street and W Broadway Street. The intersection of South High Street and Miami is signalized. At the S.R. 55 intersection, South High Street aligns with the drive to the Champaign County Community Center and is stop-controlled. Some storm sewer has been constructed by the City of Urbana, however, in several locations the existing storm is ineffective, and ponding is evident after storm events. Some minor grade revisions were performed by the City to alleviate some of the ponding concerns as a near-term solution, however the area generally still sees ponding issues. The residents along the corridor have indicated that speeding is an issue.

There is currently no direct access to the Simon Kenton Trail from South High Street. The trail can be accessed from College Way or Miami Street west of the South High Street intersection.

Most of the residences in the South High Street corridor are single family homes. Some multi-family complexes are located at the southern end of the project. The corridor has two characteristics. North of College Street the homes are older, historic homes with larger lawns, fences, brick walks, and large trees adjacent to the roadway. Several locations were identified by the City of Urbana as historic in nature and areas to avoid impacts. These included the existing serpentine brick wall at 218 W Market Street (parcel K48-25-00-01-16-002-00), properties on the National Historic Registry at 222 College Street and 335 College Street, and the house on the southeastern corner of S High Street and Water Street at 233 W Water Street (K48-25-00-01-16-079-00). Homes south of College Street are post World War II era houses with smaller lawns and with smaller or no trees adjacent to the roadway.

Urbana University is in the corridor between Powell Avenue and University Drive, with access at Reynolds Street / College Way. South High Street is considered the main gateway to the university campus. Signage for the campus currently exists at the intersection of South High Street and Miami Street, Reynolds Street and U.S. 68, and at S.R. 55 and South High Street.

Deliveries to the University typically access the campus via U.S. 68 to Reynolds Street, crossing South High Street. Buses for collegiate or other sporting events typically access the campus via S.R. 55 and South High Street to University Drive. The University has about 350 on campus students. Other students are residents from Urbana or commute to the college campus. The University athletic fields that border South High Street between University Drive and Powell Avenue are also used seasonally by community organizations for K-12 club sports.

South High Street serves as a connector to numerous pedestrian, cyclists and motorist destinations for those living or working in the corridor and at the University. At the northern end of the corridor, South High Street provides access to Miami Street, with downtown Urbana and numerous restaurants, businesses and other services several blocks to the east. At the intersection, pedestrians, cyclists and motorists can access the local soup kitchen and the UDF just to the east. To the west on Miami Street is the Depot Coffee House and the Simon Kenton Trail. At the southern end of the corridor, a hotel is proposed in the northeastern quadrant of S.R. 55 and South High Street, with several outparcels that may see future development and create destinations for pedestrian, cyclists or motorists in the South High Street corridor. At the South High Street and S.R. 55 intersection is the drive access to the Champaign County Community Center, including Bureau of Motor Vehicles, Job & Family Services, Board of Elections, County Recorder, County Auditor, County Treasurer, OSU Extension Office, County Health District, Ohio Means Jobs Champaign County, Driver Exam Station, as well as Freedom Grove.

There is on-demand transit service in the corridor that operates from 8 am to 5 pm by appointment.

Design Criteria Referenced in Study

TABLE 2 summarizes the design criteria and reference document was used in the analysis of the corridor footprint assuming a 25-mph design speed for South High Street.

TABLE 2: DESIGN CRITERIA

CRITERIA	DIMENSION	REFERENCE
Design / Posted Speed for South High Street	25 mph	Posted speed limit signs in field
Minimum Thru Lane	10'	ODOT Location & Design Manual (L&D), Volume 1 Section 300, Figure 301-4
Parking Lane Width	7'	ODOT L&D Manual, Volume 1 Section 300, Figure 301-4
Sidewalk Width	5'	ODOT L&D Manual, Volume 1 Section 300, Figure 306-2
Sidewalk Buffer (Tree lawn)	2' minimum	ODOT L&D Manual, Volume 1 Section 300, Figure 306-2
Bike Lane Width	5'	ODOT L&D Manual, Volume 1 Section 308.5.4
Buffer Between Parking and Bike Lane	2.5' (total bike lane, buffer, parking width = 14.5')	NACTO Urban Bikeway Design Guide, Conventional Bike Lanes
Calculation for Horizontal Pavement Tapers	$L \text{ (length of taper)} = (\text{Width} * \text{Speed}^2) / 60$	Section 301.1.4
Speed Hump Design	Length – 14' Height – 3.5"	Updated Guidelines for the Design and Application of Speed Humps, ITE (2011)

Traffic Analysis

Traffic volumes were collected by CCSTCC using Miovision at the intersections of U.S. 36 & High Street and S.R. 55 & High Street. Counts at the US 36 & High Street were collected on September 27, 2018 and the SR 55 intersection was collected on October 10, 2018. The AM peak hour for the intersection of US 36 & High Street was determined to be 7:30am – 8:30am and 3:30pm – 4:30pm for the PM peak hour. At the intersection of SR 55 & High Street, the AM peak hour is 6:00am – 7:00am and the PM peak hour is 2:45pm – 3:45pm. Existing AM and PM peak hour traffic counts for the two study intersections are illustrated in **FIGURE 4**. Raw traffic counts are summarized in the Appendix.

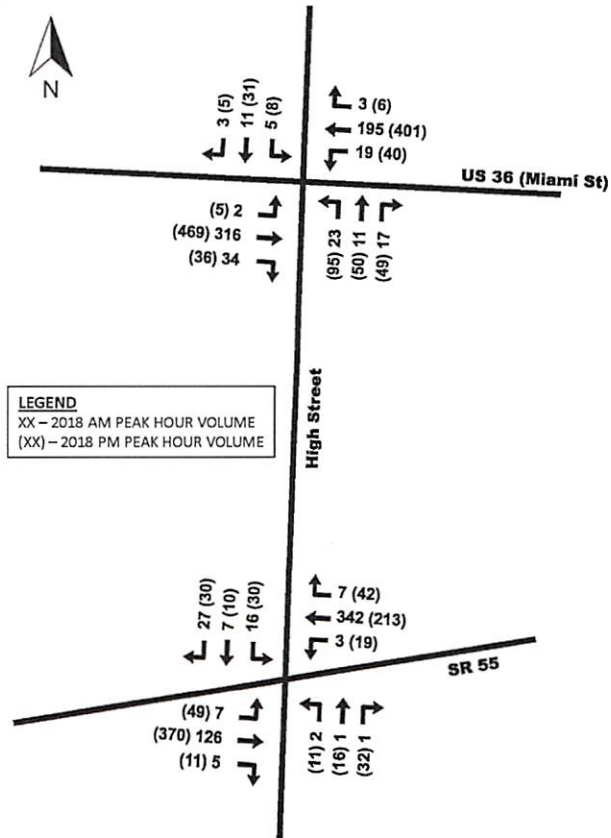


FIGURE 4: 2018 TRAFFIC VOLUMES

Traffic Forecasts

To develop the 2040 traffic forecasts, the ODOT Modeling and Forecasting Unit provided a SHIFT (Simplified Highway Forecasting Tool) analysis for U.S. 36 and S.R. 55. The SHIFT analysis uses historic traffic counts as well as the Statewide travel demand model to estimate the annual growth rate for traffic volumes. For this area, the SHIFT analysis determined that traffic volumes were declining. Per ODOT standard practice, the 2040 volumes from SHIFT were reported to be the same as the 2018, rather than show a decline. For the High Street study, a 1% annual growth rate was assumed to provide a conservative analysis. This equates to a 22% increase in traffic volumes between 2018 and 2040. **FIGURE 5** shows the 2040 traffic volumes.

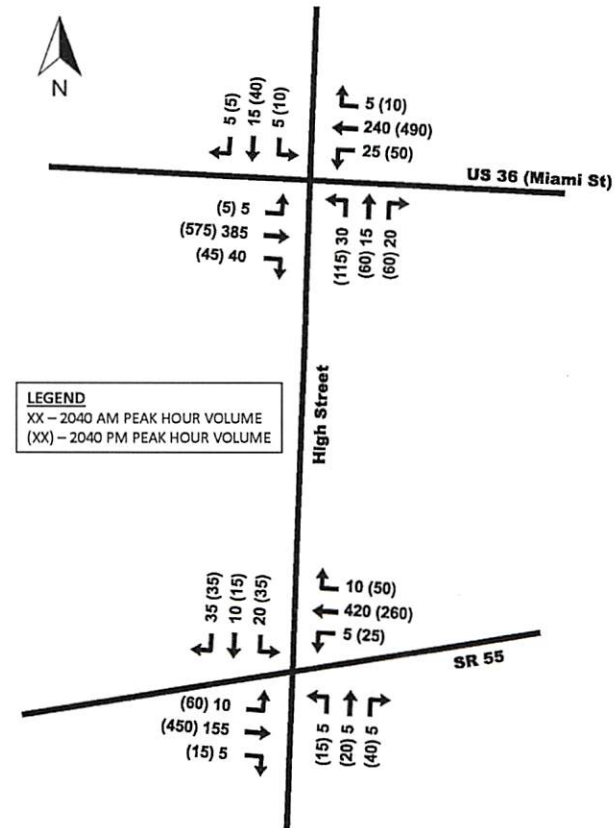


FIGURE 5: 2040 TRAFFIC VOLUMES

Capacity Analysis

Capacity analysis was conducted for the intersections of U.S. 36 & High Street and S.R. 55 & High Street for the 2018 and 2040 conditions. Analysis was conducted with Synchro 10.0 software using the HCM 2010 module. The Level of Service (LOS), delay, volume-to-capacity ratios (v/c), and 95% queue results for both the AM and PM peak hours are illustrated in **TABLE 3** for the U.S. 36 & High Street intersection, and **TABLE 4** for the S.R. 55 & High Street intersection. Output from Synchro is provided in the Appendix.

TABLE 3: HIGH STREET & U.S. 36 CAPACITY RESULTS

High Street & US 36 Existing Condition Intersection Operations													
Int.	Eastbound US 36			Westbound US 36			Northbound High Street			Southbound High Street			
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT	
AM Peak Hour													
LOS	B	B	B	B	B	B	B	B	B	B	B	B	
Delay	12.7	11.7	13.5	15.1	11.1	13.4	12.9						
v/c		0.004	0.471	0.050	0.262	0.100	0.033						
95 th Queue		3'	151'	15'	84'	29'	16'						
		B - 13.5			B - 11.4			B - 13.4			B - 12.9		
PM Peak Hour													
LOS	B	B	B	B	B	B	B	B	B	B	B	B	
Delay	15.4	15.0	16.2	19.4	13.6	16.8	13.9						
v/c		0.013	0.653	0.134	0.521	0.329	0.075						
95 th Queue		6'	237'	25'	178'	100'	30'						
		B - 16.2			B - 14.1			B - 16.8			B - 13.9		
High Street & US 36 2040 Condition Intersection Operations													
Int.	Eastbound US 36			Westbound US 36			Northbound High Street			Southbound High Street			
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT	
AM Peak Hour													
LOS	B	B	B	B	B	B	B	B	B	B	B	B	
Delay	13.2	11.8	14.1	16.3	11.0	14.3	13.6						
v/c		0.009	0.550	0.071	0.313	0.110	0.040						
95 th Queue		6'	186'	17'	101'	36'	19'						
		B - 14.0			B - 11.4			B - 14.3			B - 13.6		
PM Peak Hour													
LOS	B	B	B	C	B	B	B	B	B	B	B	B	
Delay	17.7	16.5	19.4	23.9	14.7	19.0	14.8						
v/c		0.014	0.774	0.213	0.619	0.417	0.096						
95 th Queue		6'	364'	33'	226'	128'	36'						
		B - 19.4			B - 15.5			B - 19.0			B - 14.8		

TABLE 4: HIGH STREET & S.R. 55 CAPACITY RESULTS

High Street & SR 55 Existing Condition Intersection Operations													
Int.	Eastbound US 36			Westbound US 36			Northbound High Street			Southbound High Street			
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT	
AM Peak Hour													
LOS	A	A	A	A	A	A	B	B	B	B	B	B	
Delay	1.4	0.4	0.1	12.4	12.3								
v/c		0.01	0.00	0.01	0.10								
95 th Queue		1'	0'	1'	8'								
		A - 0.4			A - 0.1			B - 12.4			B - 12.3		
PM Peak Hour													
LOS	A	A	A	C	C	C	C	C	C	C	C	C	
Delay	3.3	0.9	0.6	15.9	17.8								
v/c		0.04	0.02	0.16	0.22								
95 th Queue		3'	2'	14'	20'								
		A - 0.9			A - 0.6			C - 15.9			C - 17.8		
High Street & SR 55 2040 Condition Intersection Operations													
Int.	Eastbound US 36			Westbound US 36			Northbound High Street			Southbound High Street			
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT	
AM Peak Hour													
LOS	A	A	A	B	B	B	B	B	B	B	B	B	
Delay	1.8	0.5	0.1	13.6	14.2								
v/c		0.01	0.00	0.03	0.15								
95 th Queue		1'	0'	3'	14'								
		A - 0.5			A - 0.1			B - 13.6			B - 14.2		
PM Peak Hour													
LOS	A	A	A	C	D	D	D	D	D	D	D	D	
Delay	4.4	0.9	0.6	21.3	26.2								
v/c		0.05	0.03	0.27	0.35								
95 th Queue		4'	2'	26'	38'								
		A - 0.9			A - 0.6			C - 21.3			C - 26.2		

Reviewing the results shown in **TABLE 3**, the intersection of U.S. 36 & High Street operates at LOS B during the AM and PM peak hours in both 2018 and 2040. As shown in **TABLE 4**, the intersection of S.R. 55 & High street operates at LOS A during the AM and PM peak hours in both 2018 and 2040. This operation is acceptable, and no capacity improvements are recommended at these intersections.

Safety Analysis

Crash data from the most recent three calendar years (2015 through 2017) was obtained from ODOT's GIS Crash Analysis Tool (GCAT) through the TIMS portal. The OH-1 report for each documented crash was reviewed to correct information where necessary and to locate the crashes properly within the study area.

In the three-year study period, there were 26 reported crashes with nine (35 percent) resulting in injury. Seven crashes occurred in 2015, 10 crashes occurred in 2016, and nine crashes occurred in 2017. **FIGURE 6** illustrates the crash frequency by type of crash. A collision diagram that shows crash patterns by illustrating the approximate location of each reported crash at this intersection is provided in **FIGURE 6**.

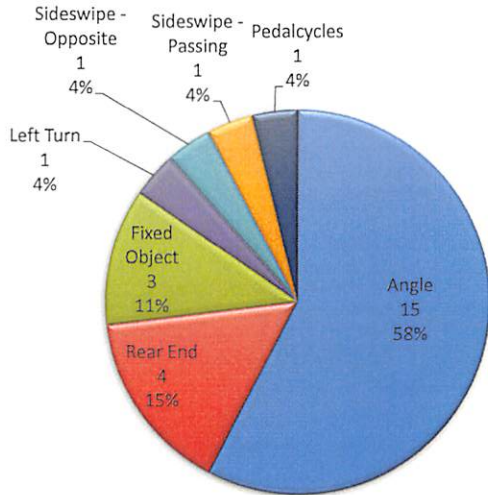


FIGURE 6: FREQUENCY OF CRASHES BY TYPE OF CRASH

through vehicle and eastbound through vehicle. There may be several factors contributing to these crashes. The northbound and southbound approaches are stop controlled while the eastbound and westbound approaches of S.R. 55 are uncontrolled. Conversely, several of the other intersections along South High Street are all-way stop-controlled with traffic stopping on all four approaches. Without a W4-4p sign (“Cross Traffic Does Not Stop”), vehicles on South High Street may not realize that traffic on S.R. 55 does not stop. Another contributing factor may be the horizontal curve along S.R. 55 between South High Street and US 68. Vehicles on South High Street and coming out of the Champaign County complex may not have clear lines of sight to vehicles coming from the east on S.R. 55 because of the curve. As a result, drivers may select an inadequate gap. Similarly, vehicles coming from the west on S.R. 55 may be traveling at higher rates of speed which results in fewer, shorter gaps.

The other angle collisions occurred throughout the study area, with no other discernable pattern to the crashes.

There were no other identified crash patterns throughout the study area. 10 crashes occurred at the signalized intersection of South High Street with US 36 (Miami Street); however, there were no discernable patterns to these crash occurrences.

The near-term recommendation at S.R. 55 is to provide the W4-4P sign (“Cross Traffic Does Not Stop”) for motorists stopped on South High Street. See **FIGURE 7** for a graphic of this sign.

The majority of the crashes within the study area were angle crashes. Crashes occurred throughout the day, with the majority of crashes (58 percent) occurring between the hours of 10:00 AM and 5:00 PM. Over 73 percent of the crashes occurred on dry pavement which indicates that adverse weather or pavement conditions were not contributing factors to crashes occurring within the study area.

Of the 15 angle collisions occurring within the study area, three (20 percent) resulted in injury. Seven of the 15 angle crashes occurred at the unsignalized intersection of South High Street with S.R. 55 (Lewis B. Moore Drive). Three of these crashes involved a southbound vehicle and an eastbound vehicle, while two crashes involved a northbound vehicle and westbound vehicle. Two crashes involved southbound left-turning vehicles with each a westbound



FIGURE 7: SIGN W4-4P

Proposed Corridor Improvements

After meeting with the City of Urbana, Urbana University, and the CCSTCC, the following potential corridor improvements were identified to address the goals and public and City concerns for South High Street.

The traffic analysis confirmed that two lanes would be adequate for the traffic in the project corridor. All existing curb within the project corridor will be replaced with new curb to meet current city standards. In the areas along the corridor where no curb is present, new curb will be placed to facilitate drainage and meet the urban collector design criteria. Continuous sidewalks conforming to a 5’ standard width is proposed within the corridor from S.R. 55 to Miami Street, creating pedestrian connectivity to the various services and amenities in the area. On-street bike facilities will be provided throughout the corridor. On street parking will be maintained in a portion of the corridor. The City staff identified the need for traffic calming elements in the corridor to slow traffic and create a corridor that would discourage cut-through traffic.

Initial Proposed Improvements

Based on the collaboration meeting with the City, University and CCSTCC, initial improvements were proposed in preparation of the January 10, 2019, public involvement meeting.

Throughout the entire corridor, new curb and 5’ sidewalk was proposed. A tree lawn was proposed where right-of-way could accommodate the tree lawn. Curb ramps were provided at all the side street intersections, at S.R. 55 on the northern leg of the intersection (crossing South High Street), and at Miami Street, meeting the Proposed Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way Guidelines (July 2011).

From S.R. 55 to College Street, the wider right-of-way and offset of the houses from the roadway facilitated a wider roadway footprint, providing two 10.5’ wide travel lanes, 5’ bike lanes and an 8’ parking lane with a 3’ buffer between the parking lane and the bike lane. A cycle track was discussed with the City. There was concern about driver expectancy and the cycle track since cyclists travel in both directions on a single side of the roadway. Drivers expect bicyclists to be traveling in the same direction as oncoming traffic and may not look in the opposite direction for cyclists in the adjacent curb lane, creating the potential for bicycle / vehicle crashes. There is also a challenge of transitioning the cycle track at S.R. 55 and at College Street and shifting the two-way travel on one side of the roadway to a “Share the Lane” movement. These concerns eliminated the cycle track option. Since parking was only necessary along one side of the roadway to address the corridor needs, parking was proposed along the eastern side of South High Street from just south of W Broadway Street to College Street, matching the limits of the current gravel parking pull offs used by residents in the corridor in existing conditions.

From College Street to Miami Street, the footprint was narrowed to avoid significant right-of-way impacts and adjacent houses. The travel lanes matched the existing conditions and were 10' wide, and the bike lanes transitioned to a "Share the Lane" condition. No parking was proposed in this stretch of the corridor. A minor alignment revision was made to improve sight distance at Water Street and South High Street. The existing house in the southeastern corner of the intersection sits adjacent to the back of sidewalk, creating an obstruction to the intersection sight distance. To mitigate this obstruction, the roadway alignment for South High Street was tapered slightly to the west, eliminating the proposed tree lawn between the sidewalk and the roadway. This taper was shifted back to the original roadway alignment just north of the intersection.

Traffic Calming Elements

While stop signs are sometimes thought of as traffic calming elements, the installation of a stop sign is not intended to slow traffic. Stop signs cannot be placed without warranting conditions based on traffic volumes, sight distance and crash records (per the Manual on Uniform Traffic Control Devices, or MUTCD). Placing stop signs at intersections that do not meet the warrants may be violated, increasing the potential for crashes. Additionally, studies have shown that stop signs are not effective for controlling or reducing midblock speeds and overuse of stop signs could cause drivers to carelessly stop or not stop. One study showed placing stop signs along a street could increase peak speeds as motorists tend to speed up between stop signs to regain perceived lost time at the sign.

The South High Street corridor does have three intersections with longstanding four-way stop control. Speeding was noted as a concern as well as cut through traffic. There are opportunities between the four-way stop controlled intersections to provide additional traffic calming that will slow traffic. With the decrease in speeds and increase in travel time, motorists looking for a quick route around downtown are likely to avoid South High Street with these additional traffic calming elements.

To provide traffic calming elements, traffic circles were proposed at Powell Avenue and College Street, and curb extensions or "bulb-outs" were proposed along the corridor from W Broadway Street to College Street at the parking lane.

Per the NACTO Urban Bikeway Design Guide, traffic circles are one of several recommended treatments to provide traffic management in an urban corridor. Traffic circles can reduce conflicts and maintain slower speeds. Motorists will need to slow before navigating around the circle. To address the potential for buses and delivery trucks in the corridor and reduce maintenance costs with landscaping, the traffic circles were proposed with a 4" rolled curb and concrete center island. The 4" height of the island is tall enough to discourage passenger cars from traversing the center island while still providing full use of the island to larger vehicles. Traffic circles do differ from roundabouts. The entry and exit geometry is not curved like a roundabout with splitter islands while vehicles enter a traffic circle in a straight line. In the South High Street corridor, placing a traffic circle at Powell Avenue provides a break in the straight stretch of roadway between two stop-controlled intersections – W Broadway Street and Thompson Street. Similarly, a traffic circle at College Street provides a traffic calming element between Thompson Street and the four-way stop at Reynolds Street. The traffic circle at College Street also provides an indicator to the motorist of a change in the corridor characteristics to the historic neighborhood in the corridor and a changing in the bicycle facilities from bike lanes to "Share the Lane" markings. The benefits of the traffic calming at College is also realized by the bicyclists as they transition from dedicated bike lanes to a shared lane situation. Pedestrians benefit from slower vehicle speeds due to the reduced severity of crashes between vehicles and pedestrians, if a crash occurs. See **FIGURE 8** for examples of traffic circles.



FIGURE 8: EXAMPLES OF TRAFFIC CIRCLES

A second traffic calming element in the S.R. 55 to College Street part of the corridor is curb bulb-outs. Located at the intersections, the bulb out extends the sidewalk into the street, ending the parking lane. The biggest benefits of the bulb out are the increased visibility of pedestrians and the shorter crossing length. Visibility is increased by bringing the pedestrians out beyond the parking lane. The physical and visual narrowing of the roadway also serves as a traffic calming measure by narrowing the roadway corridor at the intersections. These features are recommended by the NACTO Urban Design Guide where on-street parking is provided for these reasons. An example of a curb extension or bulb-out is shown in **FIGURE 9**.



FIGURE 9: EXAMPLE OF CURB EXTENSION ("BULB-OUT")

Public Involvement

The recommendations described in the previous section were presented at a public meeting held on January 10, 2019. A mailing was sent on December 20, 2018 to approximately 88 property owners that had property abutting South High Street in the corridor limits. Twenty-three attendees (including staff from City of Urbana, CSSTCC, and B&N) were present. A representative from the local media also attended. The meeting was held on the Urbana University campus from 6:00 pm

to 7:30 pm with a short presentation on the study goals and proposed recommendations at 6:30 pm. Prior to and at the end of the presentation, public comments and questions were received. The public meeting flyer, sign in sheet and comment summary can be found in the Appendix.

Below is a summary of the public comments and the response or proposed revisions provided in the proposed improvements following the comments in italics.

- Residents at the College Street / South High Street intersection voiced concerns about the jog in intersection at College Street; parking from College to Hovey is not used frequently and residents indicated it was acceptable to remove the parking to adjust the jog in the intersection.
 - *The layout was adjusted to remove the parking and minimize the jog at College.*
- A resident noted observing cars not stopping at the four-way stop at Thompson Street and suggested a potential traffic circle at this location.
 - *The intersection is currently signed as a four-way stop and will remain with that signage. Other traffic calming elements will be investigated to encourage slower speeds.*
- It was suggested to place a traffic circle at Reynolds Street
 - *The intersection is currently signed as a four-way stop and will remain with that signage. Since this is a delivery route to the University, a traffic circle is not desirable here. Other traffic calming elements will be investigated to encourage slower speeds.*
- Speeding is identified as an issue north of University Drive (also noted near Miami)
 - *Other traffic calming elements will be investigated to encourage slower speeds.*
- It was asked if street lighting be provided with the project.
 - *Street lighting will be investigated at least at the intersections.*
- One resident suggested widening High Street
 - *The R/W constraints do not permit further widening and the intent of the project is to provide traffic calming and pedestrian / bicycle elements.*
- There was concern that the proposed 5' sidewalks are too wide.
 - *Per ODOT and AASHTO standard, a 5' minimum sidewalk will be provided.*
- Fix drainage
 - *Intended as part of the recommendations.*
- Residents living south of Hovey Street stated the City should definitely provide parking.
 - *Intended as part of the recommendations.*
- A resident stated no traffic circle at College Street and S High Street (too much impact to adjacent property), and asked if a stop sign could be placed instead.
 - *Noted. A stop sign must be warranted and cannot be installed for traffic calming purposes. The traffic circle is intended to slow traffic and has a minimal footprint that fits within the intersection.*
- There was a request to eliminate the buffer along bike lane and parking.
 - *Per NACTO, a buffer is recommended to prevent the potential for "dooring" collisions (a cyclists hitting an open vehicle door). The buffer width will be investigated to confirm the width.*
- Keep bike lanes
 - *Intended as part of the recommendations.*
- A comment indicated the curb extensions were okay, but the traffic circles were not desirable.
 - *Noted. Since traffic calming is a corridor goal, traffic circles will be considered as potential calming elements.*
- Like the sidewalks

From the public meeting, the buffer between the bike lane and the parking was investigated. Per the NACTO Urban Bike Design Guide, a minimum of 14.5' is preferred for a bike lane / buffer / parking lane area. This would include a 5' bike lane, a 2.5' buffer, and a 7' parking lane. The overall footprint of this area was narrowed to reduce the 8' parking lane to 7' and the 3' buffer to 2.5', pulling in the overall roadway footprint and addressing concerns about impacts to property frontage along the eastern side of South High Street.

To address the jog at the College Street intersection, the parking was identified as not necessary by the residents and eliminated. This revision provided space within the right-of-way to shift the roadway alignment to the east based on the WS² / 60 taper rate and minimize the jog at College Street.

To address other potential traffic calming options, speed humps were investigated. NACTO's Urban Design Guide provided the following recommendation:

"Vertical speed control measures are composed of wide, slight pavement elevations that self-enforce a slower speed for motorists. Note: the type of narrow, abrupt speed bumps used in private driveways or parking lots are not recommended for public streets and are a hazard to bicyclists. Some examples of recommended speed management treatments include the following:

- *Speed humps are 3 to 4 inches high and 12 to 14 feet long, such that speeds are reduced to 15 to 20 mph. They are often referred to as "bumps" on signage and by the general public.*
- *Speed humps should be no more than 500 feet (152 m) apart or between slow points where the desired 85th percentile operating speed is between 25 and 30 mph. Institute of Transportation Engineers. (2011). Updated Guidelines for the Design and Application of Speed Humps and Speed Tables.*

'Improperly designed, speed humps and all speed bumps are dangerous for bicyclists... both 3-inch and 4-inch humps are likely to be safe for bicyclists, although the 4-inch hump should probably be used with caution where bicycle traffic is frequent or rapid. Speed humps should be located far enough from intersections that turning cyclists are no longer leaning when they encounter the hump.'

Final Proposed Improvements

Based on this information, the distances between Powell Avenue and Thompson Street and Water Street and Miami Street were investigated for locations and applications. There is about 1,200' between Powell Avenue and Thompson Street. Given the above guidance, placing one speed hump between Powell Avenue and Thompson Street would provide about 600' between the intersections and the speed hump. The spacing is over the 500' maximum spacing, but not excessively so and should still prove effective in combination with the other applications in the corridor. Between Water Street and Miami Street, Market Street is the halfway point. The speed hump isn't applicable in the middle of an intersection. A potential alternative could be a speed table at this location, however, per the NACTO and ITE guidelines, speed humps lend themselves to speeds between 25 mph – 35 mph. A speed table application may not provide the same traffic calming results as a speed hump. To avoid the intersection and provide better traffic calming with a speed hump, a location halfway between Market Street and Abbey Lane was proposed. The corridor between Miami Street and Market Street feels narrow with the proximity of the adjacent private fences, serpentine wall, and houses close to the roadway, which lends itself to a natural traffic calming effect. Market Street to Water Street is where the footprint feels like it widens, especially on the east side since the houses are further off the road. A speed hump near Market could help reinforce the slower speeds.

To address the drainage concerns, the plan did not lay out a proposed storm sewer at this level of development. The cost estimate did include costs for upgrades and reuse of the existing storm system as well new storm sewer laterals and trunk line. The main storm sewer for the area drains a portion of the corridor northerly to Water Street, flows to the west to

Walnut Street, and enters the Town Branch. A portion of the Town Branch is contained in a culvert under the residential / commercial areas and city streets before outletting in a channel to Dugan Run. The remaining portion of the project flows to the south towards S.R. 55. The proposed storm sewer improvements will need to assess the amount of flow that can be directed to the Town Branch system and not exceed the existing system capacity.

After reviewing the existing street lighting in the project corridor and the alternatives for upgrading the street lighting, it was determined that the City will plan on upgrading the street lighting by installing additional light fixtures on wooden poles with overhead service connections from the adjacent Dayton Power & Light facilities.

Using this information and the public feedback, a final recommendation was made and is summarized in the following section. Plan sheets showing the horizontal alignment are provided in the Appendix.

S.R. 55 to W Broadway Street

The proposed roadway footprint from S.R. 55 to W Broadway will include 5' sidewalks, 4' tree lawns, 5' bike lanes, and two 10.5' traffic lanes. Just south of W Broadway, a 7' parking lane and a 2.5' buffer will be provided. The 4' tree lawn was eliminated within the parking limits to avoid excessive right-of-way impacts. See **FIGURE 10** and **FIGURE 11** for proposed typical sections.

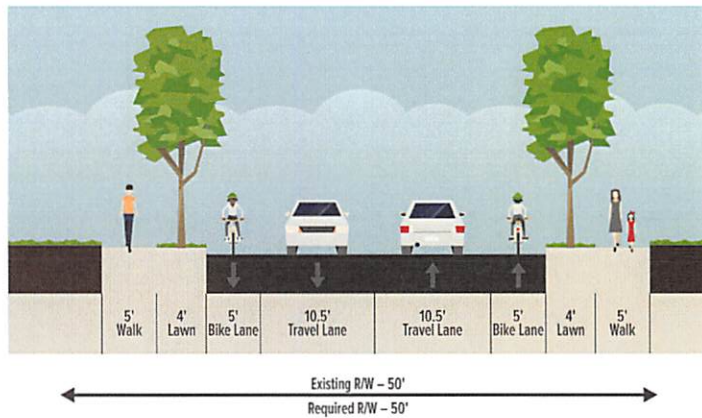


FIGURE 10: S.R. 55 TO W BROADWAY STREET – NO PARKING

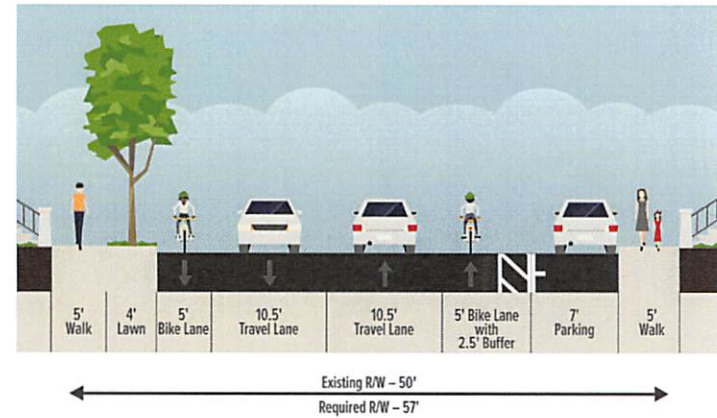


FIGURE 11: S.R. 55 TO W BROADWAY STREET – PARKING

W Broadway Street to Hovey Street

From W Broadway Street to Hovey Street, the roadway typical section includes 5' sidewalks, 5' bike lanes, two 10.5' traffic lanes, a 7' parking lane and a 2.5' buffer. This section of the corridor also included a speed hump between Powell Avenue and Thompson Street. The speed hump is shown on the plan sheets in the Appendix and is marked with chevron striping. See **FIGURE 12** for a proposed typical section.

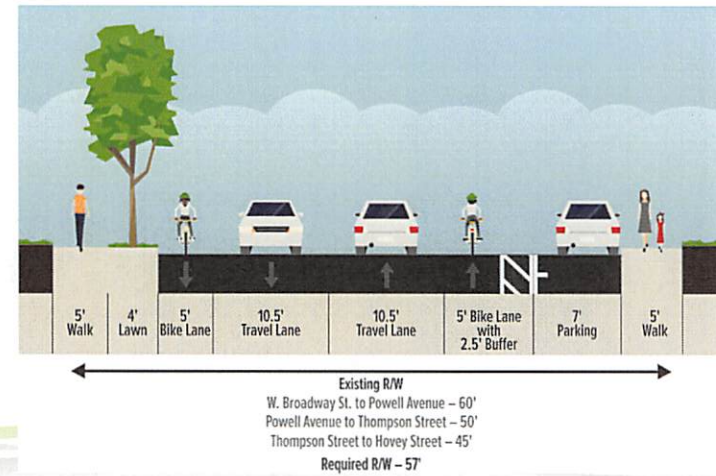


FIGURE 12: W BROADWAY STREET TO HOVEY STREET

Hovey Street to College Street

The Hovey Street to College Street typical section is similar to the W Broadway Street to Hovey Street typical section except that it eliminates the parking lane on the east side of the roadway so the alignment at College Street could be adjusted to minimize the jog in the roadway. See **FIGURE 13** for a proposed typical section.

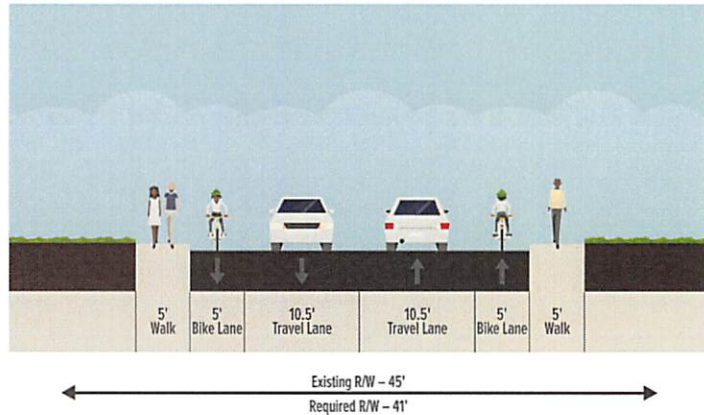


FIGURE 13: HOVEY STREET TO COLLEGE STREET

College Street to Miami Street

From College Street to Miami Street, the roadway typical section includes 5' sidewalks, a 3' minimum tree lawn where right-of-way permitted, two 10' traffic lanes, and "Share the Lane" markings. This section of the corridor also included a speed hump between Water Street and Market Street. The speed hump is shown on the plan sheets in the Appendix and is marked with chevron striping. See **FIGURE 14** for a proposed typical section.

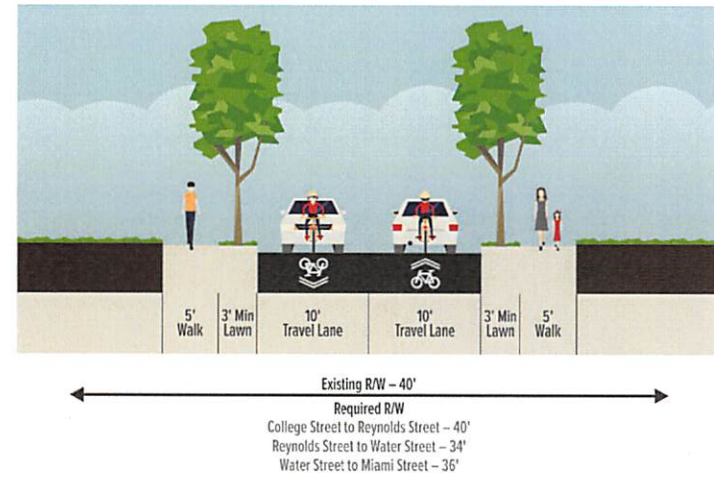


FIGURE 14: COLLEGE STREET TO MIAMI STREET

Preliminary Cost Estimate

A preliminary cost estimate was developed for the proposed final improvements. The following assumptions were made in this estimate:

- In each category, a contingency for miscellaneous items was provided. The intent of this contingency is to capture the costs for items typically identified in detail design. An overall estimate contingency was also applied for the preliminary level estimate.
- The proposed storm sewer cost assumed that a small portion of the existing storm sewer recently installed by the City of Urbana could be maintained. The costs for pipe removal and backfill of any proposed pipe trenching is captured in the contingency percentage for miscellaneous storm items.
- Post Construction Stormwater Best Management Practices – per the ODOT L&D Volume 2, projects with less than 1 acre of new impervious area located within new right-of-way do not require quantity treatment. Based on the right-of-way information provided, the proposed final improvements do require right-of-way acquisition, but the total new impervious area located within this new right-of-way is less than 1 acre. It was assumed that only quality treatment will be required. Two manufactured systems were proposed to provide the quality treatment, assuming one system will be placed at the outfall of each storm sewer trunk line.
- Various water items were included to account for impacts to the fire hydrants, water valves, and minor raising or lowering of water services and lowering of water mains.
- The pavement quantities assumed full depth pavement along the widening areas and within 2' of proposed curb replacement. A proposed build-up was provided, including 1.5" of asphalt surface course, 7.5" of asphalt concrete base, and 6" of aggregate base. It was assumed that the existing street would be milled 1.5" and resurfaced. A contingency quantity for full depth spot repairs was also provided to address spot locations of base failure within

the resurfacing area. The asphalt quantity for the speed humps assumed a 4" thickness over the width and length of the speed hump.

- Concrete drive quantities were calculated assuming a 6" depth.
- The concrete traffic circle center was assumed to be 8" thick.
- At the South High Street and Miami Street intersection, new pedestrian push buttons and pedestals were proposed at the new ramps in the southern leg of the intersection.

Costs are summarized in **TABLE 5**.

TABLE 5: ESTIMATED COSTS FOR ALTERNATIVE 1

PROPOSED FINAL IMPROVEMENT COSTS	
ROADWAY	\$321,000
DRAINAGE	\$1,032,100
WATER	\$101,500
PAVEMENT	\$1,397,000
EROSION CONTROL	\$49,600
TRAFFIC CONTROL	\$100,700
TRAFFIC SIGNAL	\$4,700
MAINTENANCE OF TRAFFIC	\$92,000
MISC. ITEMS	\$131,000
20% CONTINGENCY	\$646,000
TOTAL	\$3,875,600

Boc #1 & #2

Utility Service Company
Water Tower Maintenance Contract

Updated
2/11/2019

Year	Tower				Total
	East Lawn	Gwynne	East	West	
2015	105,639				105,639
2016	105,639	40,000			145,639
2017	105,639	132,698			238,337
2018	105,639	132,698			238,337
2019	105,639	132,698	85,432	2,500	326,269
2020	40,766	132,698	85,432	85,339	344,235
2021	40,766	132,698	85,432	85,339	344,235
2022	40,766	56,897	85,432	85,339	268,434
2023	46,881	56,897	85,432	85,339	274,549
2024	46,881	56,897	45,177	85,339	234,294
2025	46,881	59,742	47,436	46,776	200,835
2026	53,913	59,742	49,808	49,115	212,577
2027	53,913	59,742	52,298	51,571	217,523
2028	53,913	62,729	54,913	54,149	225,704
2029	62,000	62,729	57,659	56,857	239,244
2030	62,000	62,729	60,542	59,699	244,970
2031	62,000	65,865	63,569	62,684	254,118
2032	71,300	65,865	66,747	65,819	269,731
2033	71,300	65,865	70,084	69,109	276,359

BOC# 3

City of Urbana
Memorandum

Date: February 11, 2019
To: Mayor Bill Bean
From: Ryan Williams, Committee Chairman
Subject: 2019 Health Insurance Recommendation
Copy To: Chris Boettcher
Liela Anderson

On February 11, 2019 the City of Urbana Health Insurance Committee, by a vote of 9 – 0 of the members present, recommend that the City of Urbana accept the option 5 plan submitted by United Health Care for traditional and high deductible health insurance plans as presented by our broker of record, USI Insurance Services. The renewal is 5.9 percent increase over the 2018 rates.

The committee also recommends that the City continue contributing to the employees' Health Savings Account at the current amount of \$1,700 (single) and \$3,400 (family).

Delta Dental and EyeMed rates will remain the same as the 2018 rates.

The committee is dedicated to maintain health insurance costs for our fellow employees and the City of Urbana. Last year the committee recommended an 8.7 percent increase for 2018.

Respectfully submitted on behalf of the Committee,

Ryan Williams



BB 2/13/19

KB 2/13/19

CB 2/13/19



City of Urbana
Medical Plan
Benefit Outline and Cost Summary
March 01, 2019 Renewal

BOC # 3

Benefit Outline	Current				Option 5			
	Plan 1		Plan 2		Plan 1		Plan 2	
Carrier / PBM	Medical Mutual		Medical Mutual		United HealthCare		United HealthCare	
Plan Type	PPO		HSA		PPO		HSA	
Plan Name	3080-2500		2700		BCOY Rx 01		Ayy2 Rx 01	
Network	Super Med Plus		Super Med Plus		Choice Plus		Choice Plus	
Deductible (Individual / Family)	\$2,500 / \$5,000	\$2,700 / \$5,400	\$1,500 / \$3,000	\$2,700 / \$5,400				
Deductible Type	Embedded		Embedded		Embedded		Embedded	
Out-of-Pocket Maximum (Ind. / Fam.)	\$4,800 / \$9,600	\$3,600 / \$7,200	\$5,000 / \$10,000	\$4,500 / \$9,000				
Coinsurance (In)	80%		100%		80%		100%	
Wellness / Preventive Care	\$0		\$0		\$0		\$0	
Primary Care Office Visit	\$30		100%		<19: \$0 DN: \$25; N: \$25		Ded, \$25	
Specialist Office Visit	\$60		100%		DN: \$25; N: \$50		Ded, \$50	
Walk-In / Urgent Care Visit	\$75		100%		\$75		Ded, \$75	
Emergency Room	\$250		100%		\$250, then 80%		Ded, \$250	
Outpatient Lab / X-Ray	80%		100%		\$0		100%	
Complex Imaging (MRI, CAT, PET, et.al)	80%		100%		80%		100%	
Outpatient Surgical Facility	80%		100%		80%		100%	
Inpatient Hospital Facility	80%		100%		80%		100%	
Prescription OOP Max (Ind./ Fam.)	Included w/med		Included w/med		Included w/med		Included w/med	
Retail Prescription Drug Copays	\$10/\$25/\$40		Ded, \$10/\$35/\$70		\$10/\$35/\$70		Ded, \$10/\$35/\$70	
Mail Order Prescription Drug Copays	\$10/\$65/\$120		Ded, \$10/\$88/\$175		\$25/\$87.50/\$175		Ded, \$25/\$87.50/\$175	
Specialty Prescription Drugs	25% up to \$200 max		25% up to \$200 max		N/A		N/A	
Non-network Deductible (Ind. / Fam.)	\$5,000 / \$10,000	\$5,400 / \$10,800	\$5,000 / \$10,000	\$7,500 / \$15,000				
Non-network OOP Max (Ind. / Fam.)	\$10,000 / \$20,000	\$7,400 / \$14,800	\$10,000 / \$20,000	\$15,000 / \$30,000				
Non-network Coinsurance	60%		70%		50%		50%	
Rates & Total Cost	Enroll.	Premium	Enroll.	Premium	Enroll.	Premium	Enroll.	Premium
Employee	5	\$593.41	5	\$591.30	5	\$703.80	5	\$597.95
Employee + Spouse	2	\$1,305.51	19	\$1,294.85	2	\$1,548.37	19	\$1,315.50
Employee + Child(ren)	1	\$1,068.15	12	\$1,060.34	1	\$1,266.85	12	\$1,076.32
Employee + Spouse & Child(ren)	13	\$1,780.25	27	\$1,763.89	13	\$2,111.42	27	\$1,793.87
Total Employees = 84	21	\$357,474	63	\$1,054,893	21	\$423,973	63	\$1,072,015
Annual Total					\$1,412,367			
<i>Change from Current</i>								
<i>Percentage Change</i>					5.9%			
Notes	1. All Coinsurance amounts after deductible, unless otherwise noted							

BOC # 3

Employee Census - February 2019

	Traditional	H S A	Total
Employee	5	5	10
Employee+Spouse	2	19	21
Employee+Child(ren)	1	12	13
Family	13	27	40
Total Participants	21	63	84
Opt-Out			8
Total Eligible			92

PPO Plan

	Health	Dental	Vision	Total	90% City	10% Employee	Annual City Cost
Employee	\$703.80	\$22.99	\$6.82	\$733.61	\$660.25	\$73.36	\$39,614.94
Employee+Spouse	\$1,548.37	\$72.00	\$12.96	\$1,633.33	\$1,470.00	\$163.33	\$35,279.93
Employee+Child(ren)	\$1,266.85	\$72.00	\$13.64	\$1,352.49	\$1,217.24	\$135.25	\$14,606.89
Family	\$2,111.42	\$72.00	\$20.05	\$2,203.47	\$1,983.12	\$220.35	\$309,367.19
							<u>\$398,868.95</u>

H S A Plan

	Health	Dental	Vision	Total	90% City	10% Employee	Annual City Cost
Employee	\$597.95	\$22.99	\$6.82	\$627.76	\$564.98	\$62.78	\$33,899.04
Employee+Spouse	\$1,315.50	\$72.00	\$12.96	\$1,400.46	\$1,260.41	\$140.05	\$287,374.39
Employee+Child(ren)	\$1,076.32	\$72.00	\$13.64	\$1,161.96	\$1,045.76	\$116.20	\$150,590.02
Family	\$1,793.87	\$72.00	\$20.05	\$1,885.92	\$1,697.33	\$188.59	\$549,934.27
							<u>\$1,021,797.72</u>

City Cost of Premiums	\$1,420,666.67
City H S A Contributions	\$205,700.00
Total City Cost	<u>\$1,626,366.67</u>
Budgeted Amount	\$1,594,876.00
Over (under) budget	<u>\$31,490.67</u>

JANUARY 2019 PURCHASE ORDERS

PURCHASE ORDER # :	VENDOR :	PURCHASE ORDER AMOUNT :	DEPARTMENT :	EXPLANATION :	BOC APPROVAL DATE & VOTE:
25269	CHEROKEE RUN LANDFILL	\$ 6,048.76	WWTP	SLUDGE HAULED TO LANDFILL	BOC=1/3/19 (3)
25270	SOUTHEASTERN EQUIPMENT CO INC	\$ 23,700.00	SEWER MAINTENANCE	CONEQTEC HS-5711 MAN HOLE CUTTER W/ NO COLLAR CENTERING ARM OPTION	BOC=1/3/19 (3)
25271	BENJAMIN STEEL CO INC	\$ 10,158.10	SEWER MAINTENANCE	1" HR ROAD PLATES	BOC=1/3/19 (3)
25272	STANTEC CONSULTING SERVICES INC	\$ 5,000.00	AIRPORT	2019 GRIMES FIELD CONSULTING SERVICES	BOC=1/3/19 (3)
25273	SPRINGFIELD OVERHEAD GARGAE	\$ 3,260.00	CAPITAL IMPROVEMENT-CEMETERY	INSULATED GARAGE DOORS INSTALLED AT NEW CEMETERY MAINT GARAGE & LIFTMASTER DOOR OPENER	BOC=1/3/19 (3)
25274	EVANS ELECTRIC SERVICE	\$ 2,950.00	CAPITAL IMPROVEMENT-CEMETERY	QUOTE FOR 100AMP ELECTRIC SERVICE AT CEMETERY NEW MAINT BLDG	BOC=1/3/19 (3)
25276	RICOH	\$ 5,000.00	ADMINISTRATION	COPIER CHARGES	BOC=1/3/19 (3)
25283	CHAMPAIGN COUNTY IMPROVEMENT CO	\$ 50,000.00	ZONING	ECONOMIC DEVELOPMENT SUPPORT	BOC=1/3/19 (3)
25284	O.P.O.T.A.	\$ 2,700.00	POLICE	2019 BLANKET	BOC=1/3/19 (3)
25285	WHITE'S SERVICE CENTER	\$ 6,500.00	POLICE	2019 BLANKET	BOC=1/3/19 (3)
25286	A.E. DAVID COMPANY	\$ 5,000.00	POLICE	NEW HIRE UNIFORMS	BOC=1/3/19 (3)
25287	HERITAGE COOPERATIVE	\$ 20,000.00	POLICE	GASOLINE	BOC=1/3/19 (3)
25288	URBANA PARTS	\$ 2,800.00	FIRE	2019 BLANKET	BOC=1/3/19 (3)
25289	SPRINGFIELD UNIFORM	\$ 3,600.00	FIRE	2019 BLANKET	BOC=1/3/19 (3)
25290	PST SERVICES	\$ 24,500.00	FIRE	Amb Billing Fee	BOC=1/3/19 (3)
25291	EMERGENCY NETWORKING	\$ 6,000.00	FIRE	EMS REPORTS	BOC=1/3/19 (3)
25292	SOUTHEASTERN EQUIPMENT	\$ 6,000.00	FIRE	2019 BLANKET	BOC=1/3/19 (3)
25293	HERITAGE COOPERATIVE	\$ 16,000.00	FIRE	2019 BLANKET	BOC=1/3/19 (3)
25294	MR. CLEAN	\$ 3,150.00	PARK / RECREATION	PORT-A-JOHN RENTAL	BOC=1/3/19 (3)
25295	HERITAGE COOPERATIVE	\$ 9,000.00	PARK / RECREATION	GASOLINE	BOC=1/3/19 (3)
25296	WILLIAMS HARDWARE	\$ 3,000.00	PARK / RECREATION	2019 BLANKET	BOC=1/3/19 (3)
25297	NATURALLY CLEAN	\$ 10,000.00	PUBLIC WORKS	CLEANING SERVICES	BOC=1/3/19 (3)
25298	WILLIAMS HARDWARE	\$ 3,000.00	PUBLIC WORKS	MISC SUPPLIES FOR CITY BLDG	BOC=1/3/19 (3)
25299	OTIS ELEVATOR COMPANY	\$ 2,575.00	PUBLIC WORKS	ELEVATOR MAINTENANCE	BOC=1/3/19 (3)
25300	KENNEDY COTTRELL RICHARDS LLC	\$ 12,750.00	NON DEPARTMENTAL	CAFR PREP	BOC=1/3/19 (3)
25301	A1 ABLE PEST DOCTORS	\$ 5,280.00	NON DEPARTMENTAL	MOSQUITO MANAGEMENT	BOC=1/3/19 (3)
25302	COMMUNITY MERCY OCC HEALTH	\$ 3,400.00	NON DEPARTMENTAL	EMPLOYEE DRUG TESTS	BOC=1/3/19 (3)
25303	SMARTBILL	\$ 43,000.00	UTILITY BILLING	POSTAGE & PRINTING COST	BOC=1/3/19 (3)
25304	STAPLES	\$ 4,000.00	LAW	OFFICE SUPPLIES	BOC=1/3/19 (3)
25305	DYE & DOSS INSURANCE	\$ 3,725.00	AIRPORT	AIRPORT LIABILITY INSURANCE	BOC=1/3/19 (3)
25306	DBT TRANSPORTATION	\$ 4,578.00	AIRPORT	MAINTENANCE OF AWOS	BOC=1/3/19 (3)
25307	URBANA PARTS	\$ 3,000.00	STREET	2019 BLANKET	BOC=1/3/19 (3)
25308	WHITES SERVICE CENTER	\$ 3,000.00	STREET	2019 BLANKET	BOC=1/3/19 (3)
25309	SECURITY FENCE	\$ 7,000.00	STREET	2019 BLANKET	BOC=1/3/19 (3)
25310	HERITAGE COOPERATIVE	\$ 16,000.00	STREET	FUEL	BOC=1/3/19 (3)
25311	SUSAN TEHAN	\$ 19,750.00	CEMETERY	CEMETERY COORDINATOR CONTRACT	BOC=1/3/19 (3)
25312	HERITAGE COOPERATIVE	\$ 6,000.00	CEMETERY	FUEL	BOC=1/3/19 (3)
25313	WALTER DRANE	\$ 5,000.00	CAPITAL IMPROVEMENT-COUNCIL	1/1/19-12/31/19	BOC=1/3/19 (3)
25314	ALLOWAY	\$ 18,000.00	CAPITAL IMPROVEMENT-NON DEPARTMENTAL	Testing - Homeowner (landfill)	BOC=1/3/19 (3)
25315	CB&I ENVIRONMENTAL (LANDFILL)	\$ 3,000.00	CAPITAL IMPROVEMENT-NON DEPARTMENTAL	2019 BLANKET	BOC=1/3/19 (3)
25316	HEIWAY	\$ 6,000.00	CAPITAL IMPROVEMENT-STREET	Coldmix	BOC=1/3/19 (3)
25317	ASPHALT MATERIALS INC	\$ 5,000.00	CAPITAL IMPROVEMENT-STREET	Material	BOC=1/3/19 (3)
25318	PHOENIX FIRE SERVICE	\$ 15,000.00	CAPITAL IMPROVEMENT-FIRE	NEW/REPLACEMENT TURN OUT GEAR	BOC=1/3/19 (3)
25319	FIRE SAFETY SERVICES	\$ 4,000.00	CAPITAL IMPROVEMENT-FIRE	FIRE HOSE	BOC=1/3/19 (3)
25321	EARHART	\$ 2,800.00	WATER	2019 BLANKET	BOC=1/3/19 (3)
25322	EJ PRESCOTT	\$ 40,000.00	WATER	2019 BLANKET	BOC=1/3/19 (3)
25323	NEPTUNE METERS	\$ 30,000.00	WATER	2019 BLANKET	BOC=1/3/19 (3)
25324	URBANA MATERIALS	\$ 12,000.00	WATER	2019 BLANKET	BOC=1/3/19 (3)
25325	FERGUSON	\$ 6,000.00	WATER	2019 BLANKET	BOC=1/3/19 (3)
25326	RABENSTEIN	\$ 5,000.00	WATER	2019 BLANKET	BOC=1/3/19 (3)
25327	BUCKEYE POWER SALES	\$ 4,000.00	WATER	2019 BLANKET	BOC=1/3/19 (3)
25328	WILLIAMS HARDWARE	\$ 3,500.00	WATER	2019 BLANKET	BOC=1/3/19 (3)
25329	USA BLUEBOOK	\$ 5,000.00	WATER	2019 BLANKET	BOC=1/3/19 (3)
25330	MASI	\$ 7,000.00	WATER	2019 BLANKET	BOC=1/3/19 (3)
25331	BONDED CHEMICAL	\$ 20,000.00	WATER	2019 BLANKET	BOC=1/3/19 (3)
25332	HERITAGE COOP	\$ 15,000.00	WATER	FUEL	BOC=1/3/19 (3)
25333	NOVA WATER TECHNOLOGIES	\$ 8,000.00	WWTP	2019 BLANKET	BOC=1/3/19 (3)
25334	BRYCE HILL	\$ 5,000.00	WWTP	2019 BLANKET	BOC=1/3/19 (3)
25335	CHEROKEE RUN LANDFILL	\$ 5,000.00	WWTP	2019 BLANKET	BOC=1/3/19 (3)

25336	WILLIAMS HARDWARE	\$ 4,500.00	WWTP	2019 BLANKET	BOC=1/3/19 (3)
25337	FORD HALL	\$ 4,000.00	WWTP	2019 BLANKET	BOC=1/3/19 (3)
25338	ALLOWAY	\$ 28,000.00	WWTP	2019 BLANKET	BOC=1/3/19 (3)
25339	POLYDONE	\$ 50,000.00	WWTP	2019 BLANKET	BOC=1/3/19 (3)
25340	XYLEM WATER SOLUTIONS	\$ 5,000.00	WWTP	2019 BLANKET	BOC=1/3/19 (3)
25341	NCL	\$ 4,000.00	WWTP	2019 BLANKET	BOC=1/3/19 (3)
25342	IDEXX	\$ 3,000.00	WWTP	2019 BLANKET	BOC=1/3/19 (3)
25343	HERITAGE COOPERATIVE	\$ 10,000.00	WWTP	GAS/OIL	BOC=1/3/19 (3)
25344	EJP	\$ 5,000.00	SEWER MAINTENANCE	2019 BLANKET	BOC=1/3/19 (3)
25345	URBANA MATERIALS	\$ 5,000.00	SEWER MAINTENANCE	2019 BLANKET	BOC=1/3/19 (3)
25346	HERITAGE COOPERATIVE	\$ 5,000.00	SEWER MAINTENANCE	2019 BLANKET	BOC=1/3/19 (3)
25347	EAST JORDAN	\$ 10,000.00	SEWER MAINTENANCE	2019 BLANKET	BOC=1/3/19 (3)
25348	CHEROKEE LANDFILL	\$ 8,000.00	STORMWATER OPERATIONS	STREET SWEEPING DEBRIS	BOC=1/3/19 (3)
25349	BRYCE HILL	\$ 10,000.00	STORMWATER CAPITAL	STORMWATER-CONCRETE	BOC=1/3/19 (3)
25350	EAST JORDAN IRON WORKS	\$ 10,000.00	STORMWATER CAPITAL	GRATE-IRON	BOC=1/3/19 (3)
25351	OHIO LUMBER	\$ 3,000.00	STORMWATER CAPITAL	CATCH BASIN	BOC=1/3/19 (3)
25552	GRANGER	\$ 5,000.00	WATER	2019 BLANKET	BOC=1/7/19 (2)
25646	SOUTHWEST AUTOMATION	\$ 3,923.45	WWTP	BELT CONVEYOR & ROLLER	BOC=1/9/19 (3)
25657	WHITE'S AUTO GROUP	\$ 28,820.12	WWTP	2019 FORD F150	BOC=1/9/19 (3)
25658	CHEROKEE RUN LANDFILL	\$ 10,000.00	WWTP	BIOSOLIDS HAULED TO LANDFILL	BOC=1/9/19 (3)
25659	RG TRUCKING	\$ 6,400.00	WWTP	BIOSOLIDS HAULING	BOC=1/9/19 (3)
25660	CELLEBRITE INC	\$ 3,700.00	POLICE	UFED 4PC SW RENEWAL	BOC=1/9/19 (3)
25667	PLATE LOCKS	\$ 3,777.08	SEWER	PLATE LOCK KIT, PLATE LOCK CORNERS, DRILL BITS	BOC=1/16/19 (3)
25668	DINSMORE & SHOHL LLC	\$ 2,783.46	NON DEPARTMENTAL	PROFESSIONAL SERVICES RENDERED AS BOND COUNSEL	BOC=1/16/19 (3)
25688	KLEEN INC	\$ 2,787.24	WWTP	TRAFFIC CONES, FOLDING SIGN STANDS AND ROLL UP REFLECTIVE SIGNS	BOC=1/23/19 (3)
25689	CIVICA CMI	\$ 14,722.17	POLICE	ANNUAL RENEWAL OF LICENSE SUPPORT MAINTENANCE 1/1/19-12/31/19	BOC=1/23/19 (3)
25717	BURGESS & NIPLE	\$4,780.00	CAPITAL IMPROV-ADMIN	PREBID ACTIVITES-MONUMENT SQ-COST PLUS NET FEE & ON-GOING SERVICES DURING CONSTRUCTION MONUMENT SQ-MAX COMPENSATION	BOC=1/31/19 (2)
25731	BERRY DIGITAL	\$ 6,000.00	NON DEPARTMENTAL	DESIGN & BUILD WEBSITE TO INCORPORATE ADA COMPLIANCE & MOBILE "FRIENDLY" LAYOUT FOR ALL DEPT PAGES	BOC=1/30/19 (3)
25733	EJP	\$ 7,096.16	SEWER	24 IN DIAMETER HDPE MANHOLE ADJUSTING RINGS AND CAULKING GUN AND CAULKING	BOC=1/30/19 (3)

Chris Boettcher
Secretary

Resolution # 2546-19

A RESOLUTION ENACTED BY THE CITY OF URBANA, CHAMPAIGN COUNTY, OHIO, TO AUTHORIZE THE DIRECTOR OF ADMINISTRATION TO SIGN ALL DOCUMENTS REQUIRED FOR THE COMBINATION OF ODOT PID #100783 (URBAN PAVING PROGRAM) WITH ODOT PID #103793 (US 36/US 68 INTERSECTION IMPROVEMENT-MONUMENT SQUARE ROUNDABOUT), AND DECLARING AN EMERGENCY.

WHEREAS, bids were accepted by the City of Urbana for the US 36/US 68 Intersection Improvement (Monument Square Roundabout) (ODOT PID #103793) on February 7, 2019;

WHEREAS, bids were higher than anticipated due to this project being located within a dense heavily trafficked area;

WHEREAS, three bids were received that exceeded the engineer's estimate by more than 20% and all three bids were rejected in accordance with Ohio Revised Code Section 153.12 (A);

WHEREAS, the project team has determined that there may be better project coordination and better economies of scale if the ODOT Urban Paving Program (UPP) project to resurface the intersection of US 36/US 68 (Monument Square Roundabout) is combined with the larger intersection improvement project;

WHEREAS, the US 36/US 68 Intersection Improvement (ODOT PID #103793) was previously authorized by Urbana City Council Resolutions #2474-16 and #2479-16;

WHEREAS, the ODOT Urban Paving Program (UPP) project to resurface the intersection of US 36/US 68 (ODOT PID #100783) was previously authorized by Urbana City Council Resolution #2519-17;

WHEREAS, the Ohio Department of Transportation has prepared plans and an estimate for the Urban Paving Program (UPP) project (ODOT PID #100783);

NOW, THEREFORE, BE IT RESOLVED BY THE URBANA CITY COUNCIL:

SECTION ONE: The Director of Administration is hereby empowered on behalf of the City of Urbana to enter into a project agreement amendment, including applicable federal funding through ODOT, to formally combine the Urban Paving Program (UPP) project (ODOT PID #100783) into the City of Urbana's larger intersection improvement project in Monument Square (ODOT PID #103793).

SECTION TWO: Upon completion of the described Project, and unless otherwise agreed, the LPA shall: (1) provide adequate maintenance for the described Project in accordance with all applicable state and federal laws, including, but not limited to, 23 USC 116; (2) provide ample financial provisions, as necessary, for the maintenance of the described Project; (3) if necessary, maintain the right-of-way, keeping it free of obstructions; and (4) if necessary, hold said right-of-way inviolate for public highway purposes.

SECTION THREE: This resolution is hereby declared to be an emergency measure to take effect and be in force immediately upon passage to protect and promote public safety by allowing for the rebid process for this project to commence so that bids can be received and a project contractor can be selected for this road construction project.


Passed: _____

Council President

Attest: _____
Council Clerk

This Resolution approved by me this ____ day of _____, 20__.

Mayor

Department requesting: Community Development		Personnel: D. Crabill	Director of Law review 
Expenditure? Y (N)	Emergency? Y (N)	Public Hearing? Y (N)	
Readings required: (1) 2 3		If yes, dates advertised:	
First reading date: February 19, 2019	Second reading date:	Third/Final reading date:	

Anticipated effective date if approved: February 19, 2019